

AGENDA
Municipal Services Committee
July 27, 2015
6:30 P.M. – Council Chambers

- 1. Call to Order & Roll Call**
- 2. Establishment of Quorum**
- 3. New Business**
 - a. Water Use Restrictions** – Amending City Code Section 6C-3-9 Restricting Watering Gardens, Lawns, Trees, Shrubs or Other Outdoor Plans
 - b. Plumbing Code** – Amending City Code Section 4-1-7-3 – Amendments to Plumbing Code
 - c. Rolling Knolls Subdivision, f/k/a Manning Woods Subdivision, 2100 Manning Road** – Petitioner Seeks Final Plat of Subdivision and Final Engineering Approval for a 26-lot, Single-Family Residential Subdivision
 - d. 951 N. Frontage Road, WoodSpring Suites f/k/a Value Place Hotel** - Petitioner Seeks Rezoning, Special Use and Variations Related to a Proposed Hotel
 - e. Resolution** – Accepting the Unit Price Proposal from Beary Landscaping for the Purchase and Installation of the 50/50 Parkway Tree Program, 75th Street Tree Planting, and Wilton Court Special Planting Amount to be Determined
 - f. Motion** – Authorizing the Expenditure of \$25,900 from the Emerald Ash Borer Round Two Treatment Program Fund for the Removal of Dead Ash Trees
 - g. Resolution** – Accepting a Storm Sewer Easement from the following property: 214 Janet Avenue – PIN 09-26-103-031
 - h. Minutes** – May 26, 2015 Municipal Services Committee
– June 22, 2015 Municipal Services Committee
- 4. Director’s Report**
- 5. Next scheduled meeting** – Monday, August 24, 2015
- 6. Adjournment**

AGENDA MEMO
MUNICIPAL SERVICES COMMITTEE
MEETING DATE: July 27, 2015

Issue Statement

Water Use Restrictions: Amending City Code Section 6C-3-9 restricting watering gardens, lawns, trees, shrubs or other outdoor plans.

Discussion

City Code currently restricts watering gardens, lawns and other outdoor plants between during certain times of the year, during certain hours of the day. The Illinois Department of Natural Resources (IDNR) is requiring municipalities on Lake Michigan water to amend local ordinances to be consistent with updated Lake Michigan Water Allocation Rules and Regulations. The updated regulates are intended to conserve Lake Michigan water.

Below is current and amended language required by the IDNR:

City Code Section 6C-3-9:

Current language:

During the period of time from May 15 to September 15 of each year, water from the City water system shall not be used for water or sprinkling of gardens, lawns, trees, shrubs or other outdoor plants between the hours of eleven o'clock (11:00) A.M. and seven o'clock (7:00) P.M. This restriction shall not prohibit the watering of newly planted seed or the laying of sod for a yard area.

Amended language:

During the period of time from May 15 to September 15 of each year, water from the City water system shall not be used for water or sprinkling of gardens, lawns, trees, shrubs or other outdoor plants between the hours of eleven o'clock (11:00) A.M. and seven o'clock (7:00) P.M. *Watering is not permitted on consecutive days, odd numbered addresses are restricted to watering on odd numbered dates, even numbered addresses are restricted to watering on even numbered dates.* This restriction shall not prohibit the watering of newly planted seed or the laying of sod for a yard area *less than 3 months old. New/replacement sprinkler systems shall be equipped with a WaterSense labeled irrigation controller and shall be in compliance with Section 2.5(g) of the Illinois Plumbing License Law [225 ILCS 320].*

Attached are letters from the IDNR notifying the City of the updated regulations.

Staff Findings/Recommendations

Staff recommends the Committee recommend approval of the proposed amendment to City Code Section 6C-3-9.

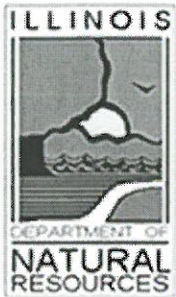
Decision Mode

Municipal Services Committee: July 27, 2015

Michael Griffith

From: Adkins, Cara N <Cara.N.Adkins@illinois.gov>
Sent: Thursday, May 07, 2015 10:06 AM
To: Adkins, Cara N
Cc: Casey, James
Subject: IDNR Water Allocation Permittees Ordinance Update- Due September 30th
Attachments: LettertoPermitteesAnnouncingFinalRules2015.pdf; IDPH Draft Ordinance Instructions_4.2015.pdf; IDPH Draft ordinance submittal form_July 2014.pdf

Due 9-30-15
to IDNR



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

Bruce Rauner, Governor
Wayne A. Rosenthal, Director

May 7, 2015

Dear Lake Michigan Water Allocation Permittees,

In a letter dated March 31, 2015 (attached), the Department requested that all Lake Michigan Water Allocation Permittees adopt and submit revised ordinances/building codes consistent with the Department's recently updated Lake Michigan Water Allocation Regulations; in particular, Section 3730.307 (c) subparts 4 and 8. The revised language includes a requirement that new and replacement plumbing fixtures and irrigation controllers on sprinkler systems be a labeled WaterSense product, as specified by the USEPA.

IDNR has coordinated with the Illinois Department of Public Health (IDPH) to ensure our rules are consistent with IDPH's rules implementing the Illinois Plumbing Code. IDNR and IDPH have crafted sample ordinance language that will meet both requirements. The sample ordinance language reads as follows:

Pursuant to 17 Ill. Admin. Code 3730.307(c)(4) and subject to the Illinois Plumbing Code (77 Ill. Admin. Code 890) and the Lawn Irrigation Contractor and Lawn Sprinkler System Registration Code (77 Ill. Admin. Code 892), be it hereby ordained that in the City/Village/County of XXXXX, all new plumbing fixtures and irrigation controllers installed after the effective date of this ordinance shall bear the WaterSense label (as designated by the U.S. Environmental Protection Agency WaterSense Program), when such labeled fixtures are available.

Prior to adopting this ordinance, you must first submit it to the IDPH for their approval. An IDPH submittal form and a description of the approval process are attached for your use. If your proposed ordinance is consistent with the substance in the sample ordinance above, it should receive IDPH approval.

In addition, all permittees are still **required to adopt** and submit a second ordinance consistent with the Department's revised language concerning non-essential outside water use (Section 3730.307 (c)(8)):

8) The adoption of ordinances that restrict non-essential outside water uses to prevent excessive, wasteful use. These shall provide that unrestricted lawn sprinkling will not be allowed from May 15 through September 15 or each year by requiring, as a minimum, that lawn sprinkling shall not occur on consecutive days nor shall any lawn sprinkling occur during at least a 6 hour period in the middle of the day (i.e., 10 a.m. through 4 p.m., noon to 6 p.m.) when evapotranspiration is at its highest. New lawns (less than 3 months old) may be exempted from this provision.

Adoption of an ordinance consistent with this language will not require prior approval of IDPH.

I apologize for any confusion and additional work this extra step will cause. All Lake Michigan water allocation permittees that are units of local government should submit their revised ordinances to the Department by September 30, 2015. If you have any questions, please contact Cara Adkins at 312.814.8579 or cara.n.adkins@illinois.gov.

James P. Casey
IDNR/OWR
Lake Michigan Management Section
160 N. LaSalle Street, Suite S-703
Chicago, IL 60601
(312) 793-5947
(312) 793-5968 fax
James.casey@illinois.gov

Please return this form, Attn: Cara Adkins, IDNR/OWR | cara.n.adkins@illinois.gov
Office of Water Resources | 160 N. LaSalle Street, Suite S-703 | Chicago, Illinois 60601
Office: 312.814.8579 | Fax: 312.793.5968

AGENDA MEMO
MUNICIPAL SERVICES COMMITTEE
MEETING DATE: July 27, 2015

Issue Statement

Plumbing Code: Amending City Code Section 4-1-7-3: Amendments to Plumbing Code.

Discussion

The City has adopted the 2004 Illinois State Plumbing Code. The Illinois Department of Natural Resources (IDNR), in coordination with the Illinois Department of Public Health, is requiring municipalities on Lake Michigan water to amend local ordinances to be consistent with updated Lake Michigan Water Allocation Rules and Regulations. The updated regulates are intended to conserve Lake Michigan water.

The following language needs to be adopted:

All new plumbing fixtures and irrigation controllers installed after the effective date of this ordinance shall bear the Water Sense label (as designated by the U.S. Environmental Protection Agency WaterSense Program), when such labeled fixtures are available.

Attached are letters from the IDNR notifying the City of the updated regulations.

Staff Findings/Recommendations

Staff recommends the Committee recommend approving the proposed amendment to City Code Section 4-1-7-3.

Decision Mode

Municipal Services Committee: July 27, 2015



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
www.dnr.illinois.gov

Bruce Rauner, Governor
Wayne A. Rosenthal, Director

March 31, 2015

To: Lake Michigan Water Allocation Permittees

From: Daniel Injerd, Chief, Lake Michigan Management

Subject: Updating Local Ordinances to be Consistent with the New Water Allocation Rules

The amendments to the Lake Michigan Water Allocation Rules and Regulations became final and effective on November 18, 2014. They have been posted to our website:

<http://www.dnr.illinois.gov/WaterResources/Pages/LakeMichiganWaterAllocation.aspx> and can be viewed or downloaded as a pdf file so that you can print a copy for your use.

Throughout the rather long and contemplative process to update the Department's administrative rules governing the Lake Michigan water allocation program, our goal has been to have these rules support our collective efforts to manage and conserve our Lake Michigan water resource.

The updated Lake Michigan Water Allocation Rules and Regulations made several changes to Section 3730.307 Conservation Practices and Other Permit Conditions. Section 3730.307(c) contains the list of conservation practices that domestic permittees must adopt in an ordinance/building code as evidence of compliance. The updated rules modified two provisions, subparts 4 and 8, that will require domestic permittees to update their ordinances/building codes to be consistent with the new language. These provisions read as follows:

4) The adoption of ordinances requiring that new and replacement plumbing fixtures be a labeled WaterSense product, as specified by USEPA.

8) The adoption of ordinances that restrict non-essential outside water uses to prevent excessive, wasteful use. These shall provide

that unrestricted lawn sprinkling will not be allowed from May 15 through September 15 of each year by requiring, as a minimum, that lawn sprinkling shall not occur on consecutive days nor shall any lawn sprinkling occur during at least a 6 hour period in the middle of the day (i.e., 10 a.m. through 4 p.m., noon to 6 p.m.) when evapotranspiration is at its highest. New lawns (less than 3 months old) may be exempted from this provision. In addition, new/replacement sprinkler systems shall be equipped with a WaterSense labeled irrigation controller and shall be in compliance with Section 2.5(g) of the Illinois Plumbing License Law [225 ILCS 320].

The new language in Subpart 4 replaces flow limits on specific plumbing fixtures. WaterSense labeled plumbing products are readily available at all hardware/plumbing supply outlets, and have become the accepted standard nationwide.

The new language of Subpart 8 provides additional guidance for permittees to include in a lawn sprinkling ordinance.

All Lake Michigan water allocation permittees that are units of local government will need to submit their revised ordinances to the Department to demonstrate compliance with these two revised subparts. Revised ordinances should be submitted to the Department by September 30, 2015. If you have any questions please feel free to contact Cara Adkins at 312.814.8579 or cara.n.adkins@illinois.gov. Revised ordinances can be submitted either electronically or as hard copy to:

Ms. Cara Noel Adkins
Illinois Department of Natural Resources
160 N. LaSalle St., Suite S-703
Chicago, IL 60601
312|814.8579
Cara.N.Adkins@illinois.gov

Michael Griffith

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May 7, 2015

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IDNR has coordinated with the Illinois Department of Public Health (IDPH) to ensure our rules are consistent with IDPH's rules implementing the Illinois Plumbing Code. IDNR and IDPH have crafted sample ordinance language that will meet both requirements. The sample ordinance language reads as follows:

Pursuant to 17 Ill. Admin. Code 3730.307(c)(4) and subject to the Illinois Plumbing Code (77 Ill. Admin. Code 890) and the Lawn Irrigation Contractor and Lawn Sprinkler System Registration Code (77 Ill. Admin. Code 892), be it hereby ordained that in the City/Village/County of XXXXX, all new plumbing fixtures and irrigation controllers installed after the effective date of this ordinance shall bear the WaterSense label (as designated by the U.S. Environmental Protection Agency WaterSense Program), when such labeled fixtures are available.

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In addition, all permittees are still required to adopt and submit a second ordinance consistent with the Department's revised language concerning non-essential outside water use (Section 3730.307 (c)(8)):

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Adoption of an ordinance consistent with this language will not require prior approval of IDPH.

I apologize for any confusion and additional work this extra step will cause. All Lake Michigan water allocation permittees that are units of local government should submit their revised ordinances to the Department by September 30, 2015. If you have any questions, please contact Cara Adkins at 312.814.8579 or cara.n.adkins@illinois.gov.

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AGENDA MEMO
MUNICIPAL SERVICES COMMITTEE
MEETING DATE: July 27, 2015

Issue Statement

PZC 2015-05: Rolling Knolls Subdivision, f/k/a Manning Woods Subdivision, 2100 Manning Road: Petitioner seeks final plat of subdivision and final engineering approval for a 26-lot, single-family residential subdivision.

Applicable Regulations: Subdivision Regulations: Section 5B-1-6: Final Plat Procedures and Requirements.

General Information

Petitioner: Paul Swanson
2100 Manning Road, LLC
401 E. Prospect Avenue
Mt. Prospect, IL 60056

Property Owner: Philip and Denise Gardner Trust
2100 Manning Road
Darien, IL 60561

Property Location: 2100 Manning Road

PIN: 09-29-400-010

Existing Zoning: R-1 Single-Family Residence

Existing Land Use: Single-family residence

Proposed Zoning: R-2 Single-Family Residence

Proposed Land Use: Single-family detached homes, 26 lots

Comprehensive Plan Update: Low Density Residential

Surrounding Zoning and Land Use:

North: B-3 General Business District: Darien Towne Center

East: R-2 Single-Family Residence: single-family homes

South: R-2 Single-Family Residence: single-family homes

West: B-3 General Business District and R-2 Single-Family Residence: Darien Towne Center and single-family homes

Size of Property: 7.6 acres

Floodplain: There is not floodplain on the property; however, at the northeast corner of the property there is a wetland on and adjacent to the property.

- Natural Features: Trees and brush cover the property, trees are generally in poor condition.
- Transportation: Property has frontage on Manning Road. Subdivision provides a new street.
- History: The City Council granted preliminary plat approval for a 26-lot residential subdivision for single-family detached home.

Documents Submitted

This report is based on the following information submitted to the Community Development Department by the Petitioner:

1. Final Plat of Subdivision, 1 sheet, prepared by R.E. Allen and Associates, Ltd., most recent revision dated July 8, 2015.
2. Final Engineering, 16 sheets, prepared by Designtek Engineering, Inc., most recent revision dated April 24, 2015.
3. Landscape Plan, 1 sheet, prepared by IRG Ives/Ryan Group, most recent revision dated June 3, 2015.

Planning Overview/Discussion

The petitioner is seeking final plat of subdivision approval for a 26-lot residential subdivision for detached, single-family home.

The City Council approved the preliminary plat of subdivision in December 2014. Preliminary plat approval included the following approvals:

1. Property rezoned from R-1 to R-2 Single-Family Residence.
2. Reduced minimum lot area from 10,000 to 6,679 square feet for one lot and to 7,200 square feet for 25 lots.
3. Reduced minimum interior lot width from 75 feet to 55.66 feet for one lot and to 60 feet for 25 lots.
4. Reduced minimum corner lot width from 85 feet to 62.36 feet.
5. Reduced minimum front yard setback from 35 feet to 25 feet.
6. Reduced interior side yard setback from 10 feet to 5.66 feet for one lot.
7. Reduced minimum minor street right-of-way width from 66 feet to 55 feet.
8. Reduced minimum minor street pavement width from 30 feet to 27 feet.
9. Reduced minimum street curve centerline radius from 150 feet to 42.5 feet.
10. Reduced minimum interior side yard utility easement from 6 feet to 5.66 feet for one lot.

Preliminary approval included the following conditions:

1. A Development Agreement between the City and the developer addressing the following:

- a. The front façade of each home shall have brick and/or stone pursuant to the following:
 - i. 1-story home façade shall be a minimum of 100% brick and/or stone.
 - ii. 2-story home façade shall be a minimum of brick and/or stone to the 2nd floor line.
 - iii. A 100% brick and/or stone front elevation shall be offered.
- b. Each home shall not exceed a Floor Area Ratio (FAR) of 0.5. FAR is the ratio between the square footage of the home and the lot. Example: for a 7,200 square foot lot, the home cannot exceed 3,600 square feet, $3,600/7,200 = 0.5$
- c. The developer shall sell off all of the lots to one home builder who will construct all of the homes within the subdivision.
- d. "Day One" landscape screening shall be planted along the entire east property line of Lots 12-17. A landscape plan shall be submitted.

Staff, developer, builder and residents have met to discuss and review a landscape plan for the east side of the development. The landscape plan is incorporated into the final approval.

Final plat approval does not require a public hearing. However, consideration by the Planning and Zoning Commission is required. The final plat is required to be submitted within 1-year of preliminary plat approval, the final plat is required to retain the design characteristics of the approved preliminary plat. However, the Mayor and City Council may require changes as are deemed necessary in the interest of the needs of the City, Section 5B-1-6 of Subdivision Regulations.

The plan includes a sidewalk along one side of the new street, along the north side of Manning Road fronting the subdivision with a crosswalk to the south side of Manning Road at Brunswick Road. This crosswalk will have to comply with the U.S. Department of Transportation's Manual on Uniform Traffic Control Devices. Staff recommended extending a sidewalk along the north side of Manning Road to Fairview Avenue. However, per the City Attorney, that request is beyond the development property and therefore the City cannot require it.

In a letter dated July 8, 2015, Dan Lynch, PE, Christopher B. Burke Engineering, Ltd., indicates the plans are in substantial compliance with City Code subject to conditions. Letter accompanies this memo.

County certification of the plans related to the wetland impacts is still outstanding and must be secured before construction can occur.

City staff has finalized the Development Agreement with the developer as required by the City Council's preliminary plat/plan approval.

Staff Findings/Recommendations

Staff finds the final plans are in substantial compliance with the preliminary plans approved by the City Council. Therefore, staff recommends the Planning and Zoning Commission make the following recommendation approving the plans, subject to conditions:

Based upon the submitted petition and the information presented, the request associated with PZC 2015-05 is in conformance with the standards of the Darien City Code and, therefore, I move the Planning and Zoning Commission approve the petition subject to the following conditions:

- 1. Address comments noted in letter from Dan Lynch, PE, Christopher B. Burke Engineering, Ltd., dated July 8, 2015.**

Planning and Zoning Commission Review – July 15, 2015

The Planning and Zoning Commission considered this matter at its meeting on July 15, 2015. The following members were present: Beverly Meyer – Chairperson, Andrew Kelly, Ronald Kiever, John Laratta, John Lind, Pauline Oberland, Kenneth Ritzert, Dan Gombac – Director, Michael Griffith – Senior Planner and Elizabeth Lahey – Secretary.

Absent: Louis Mallers, Raymond Mielkus.

Michael Griffith, Senior Planner, reviewed the agenda memo, noted the final plans are consistent with the preliminary plans approved by the City Council. He noted the engineering review letter from the City Engineer.

Paul Swanson, the petitioner, and John Berry, MacNaughton Builders, were both present.

Commissioner Laratta asked if the sidewalk to Fairview was included in the plan.

Dan Gombac, Director, stated no. He stated while staff recommended it, the City Attorney advised the City could not require it since it is beyond the parcel being developed. He noted the plan calls for a mid-block cross-walk at Burnswick, stating proper markings and signage will have to be installed per the Federal highway manual.

Chairperson Meyer asked if the detention basins would be wet or dry.

Mr. Gombac stated they would be semi-moist with native plantings as required by DuPage County to meet water quality standards.

Commissioner Ritzert voiced objection to the lot sizes, stating they were too small.

There were no comments from the public.

Without further discussion, Commissioner Kiefer made the following motion, seconded by Commissioner Kelly:

Based upon the submitted petition and the information presented, the request associated with PZC 2015-05 is in conformance with the standards of the Darien City Code and, therefore, I move the Planning and Zoning Commission approve the petition subject to the following conditions:

- 1. Address comments noted in letter from Dan Lynch, PE, Christopher B. Burke Engineering, Ltd., dated July 8, 2015.**

Upon a roll call vote, THE MOTION CARRIED by a vote of 6-1.

**Aye: Meyer, Kelly, Kiefer, Laratta, Lind, Oberland
Nay: Ritzert
Absent: Mallers, Mielkus**

Decision Mode

Planning and Zoning Commission: July 15, 2015
Municipal Services Committee: July 27, 2015



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 West Higgins Road Suite 600 Rosemont, Illinois 60018 TEL (847) 823-0500 FAX (847) 823-0520

July 8, 2015

City of Darien
1702 Plainfield Road
Darien, IL 60561

Attention: Mike Griffith

Subject: Manning Woods Subdivision
(CBBEL Project No. 950323H174)

Dear Mike:

As requested, we have reviewed the revised plans and stormwater submittal for the aforementioned development prepared by DesignTek Engineering, Inc. The plans are dated June 25, 2015 and the stormwater submittal is dated June 26, 2015. In our opinion, the plans, plat, and stormwater submittal are in substantial compliance with City Code subject to the following items:

1. Although some of the lots have storm sewer stubs for the future homes, all lots must have a stub. All lots have a storm sewer either in the right of way, rear yard, or side yard from which to provide a stub. The stub is for connection of the sump pump discharge, and Lots 1, 17, and 18 should also have the downspouts connected.
2. The final stormwater submittal must include all tabs. Some tabs were omitted as they have not changed from previous submittals, and others, such as the engineer's opinion of cost will have to be updated based on the final plans. The DuPage County letter certifying the wetland aspects of the development must be included in the appropriate tab.

Prior to recording the plat, the following must be obtained:

1. A letter of credit as required by City Code.
2. Payment of school and park impact fees as required by City Code.
3. Issuance of all outside agency permits including, but not limited to DuPage County and IEPA.

If you have any questions, please feel free to contact me.

Sincerely,

Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department

Cc: Dan Gombac – City of Darien

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DEVELOPMENT AGREEMENT
(ROLLING KNOLLS SUBDIVISION)

This Development Agreement (“Agreement”) is entered into this ___ day of _____, 2015, by and between the City of Darien, DuPage County, Illinois (“City”), McNaughton Development, Inc., 11S220 Jackson Street, Burr Ridge, Illinois 60527 (“Developer”).

RECITALS

A. Developer is the Contract-Purchaser and Developer of the “Subject Property” as defined in Ordinance No. O-38-14.

B. Ordinance No. O-38-14 calls for the City and the “Developer of the Subject Property” to enter into a Development Agreement containing certain terms set forth in said Ordinance.

C. Subsequent to the adoption of Ordinance No. O-38-14, representatives of the City and Developer have met with neighboring property owners who reside on Wilton Road (the “Wilton Residents”) to address potential adverse impacts which the development of the Subject Property might have on the Wilton Residents’ quiet enjoyment of their homes.

D. As a result of these meetings, Developer has agreed to take certain actions to mitigate any such adverse impacts.

E. The City and Developer agree that the terms set forth herein below will add to the value of the Subject Property and avoid unreasonable or adverse impacts on the properties of the Wilton Residents.

Accordingly, it is hereby agreed between the City and Developer as follows:

Section 1. Construction and Building Façade Requirements. Notwithstanding anything to the contrary appearing in the Darien City Code:

- A. Each one-story home shall be constructed of 100% brick and/or stone frontfaçade.
- B. Each two-story home shall be constructed with a brick and/or stone front façade at least to the second floor line.
- C. Developer shall offer a 100% brick and/or stone front elevation for the two-story homes.
- D. Maximum Floor Area Ratio (FAR) shall be 0.5
- E. Developer acknowledges that it is purchasing the entire Subject Property and will construct all of the homes within the Subdivision.

Section 2. Landscape Impact Fee; Security For Potential Damage To City Streets; Model Home Construction.

A. Developer agrees that at the time Developer posts its letter of credit or other security for public improvements, Developer will also deposit a Landscape Impact Fee in the amount of \$10,000 with the City for purposes of allowing the City to plant appropriate trees or other landscape materials on the properties of the Wilton Residents. For reference purposes, the addresses of the Wilton Residents are identified on **Exhibit 1** attached hereto and made a part hereof. The required landscaping along the Subdivision’s eastern property line will be completed prior to the issuance of any occupancy permits.

B. Developer’s Letter of Credit or other security shall include the amount of \$21,500.00 in order to secure the City in the event of damage to Manning Road or Lyman Road specifically attributable to Developer’s construction activity. This security may be released upon the completion of Developer’s mass earthwork activities and the certification of the City Engineer that there has been no damage to either Manning Road or Lyman Road specifically attributable to Developer’s construction activities.

C. Developer may construct model homes during the mass excavation phase.

Section 3. Homeowner Covenants. Developer agrees to establish “Covenants, Conditions and Restrictions” substantially in the form of **Exhibit 2** attached hereto and made a part hereof.

Section 4. Successors and Assigns. This Agreement shall be binding on the parties and their respective successors and assigns.

Section 5. Notices. Notices shall be in writing, either personally delivered, or through a nationally-recognized private courier (Fed Ex; UPS) to the following addresses and addressees:

If to Developer:

McNaughton Development, Inc.
11S220 Jackson Sreet
Burr Ridge, Illinois 60527
Attention: Paul McNaughton, Jr.

If to the City:

City of Darien
1702 Plainfield Road
Darien, Illinois 60461
Attention: Bryon Vana, City Administrator

With a Copy to:

John B. Murphey
Rosenthal, Murphey, Coblentz & Donahue
30 N. LaSalle Street, Suite 1624
Chicago, Illinois 60602

Section 6. Amendments. Any amendment to this Agreement shall be in writing and approved by the City’s corporate authorities.

Section 7. **Recordation.** A copy of this Agreement or a Memorandum hereof shall be recorded in the Office of the DuPage County Recorder of Deeds against the Subject Property.

CITY:

DEVELOPER:

CITY OF DARIEN

McNAUGHTON DEVELOPMENT, INC.

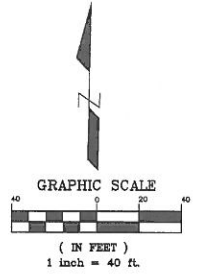
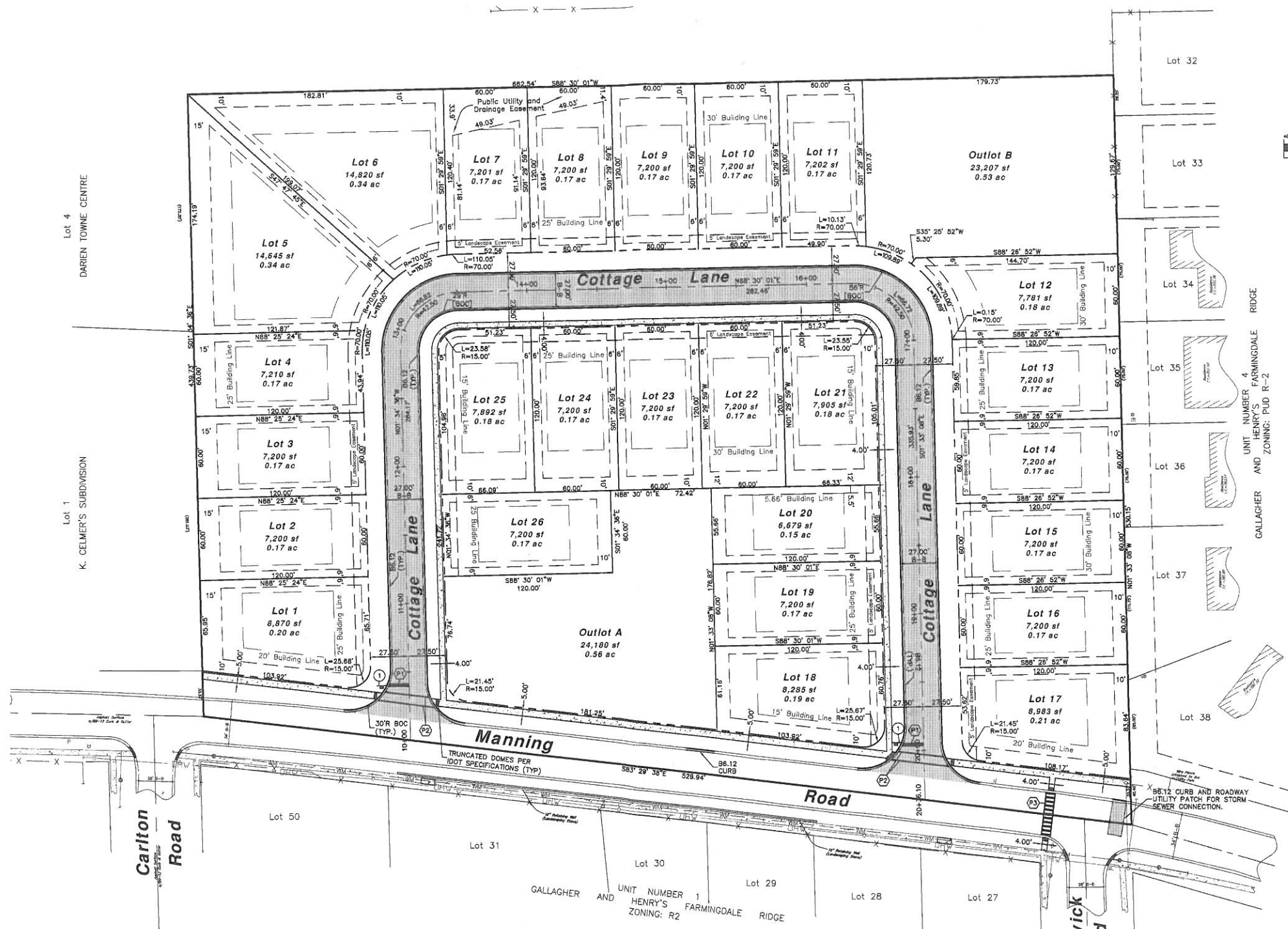
By: _____

By: _____

Its: _____

Its: _____

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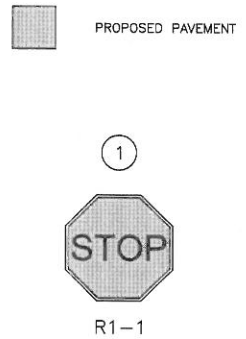


NOTES:

1. THE PLACEMENT AND SPACING OF TEMPORARY AND PERMANENT TRAFFIC CONTROL SIGNS SHALL CONFORM WITH THE LATEST EDITION OF THE IDOT STANDARD SPECIFICATIONS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, AND BE COORDINATED WITH THE CITY OF DARIEN.
2. THE DEVELOPER TO COORDINATE WITH THE CITY OF DARIEN THE LOCATION OF "NO PARKING THIS SIDE OF STREET SIGNS" WITH APPROPRIATE MUTCD DESIGNATION AND THE STREET NAME SIGNS AT THE INTERSECTIONS.
3. ALL PAVEMENT STRIPING SHALL BE THERMOPLASTIC PER IDOT SPECIFICATIONS.
4. PER MUTCD SPECIFICATIONS, THE STOP BAR TO CROSSWALK SHALL BE A MINIMUM OF 4 FEET APART, EDGE TO EDGE.
5. THE SIDEWALK ALONG MANNING ROAD IS 5 FEET WIDE. THE SIDEWALK ALONG COTTAGE LANE IS 4 FEET.
6. THE OUTLOTS SHALL BE INCLUDED IN A STORM WATER EASEMENT AND WILL BE OWNED AND MAINTAINED BY THE HOMEOWNERS ASSOCIATION.

PAVEMENT MARKING LEGEND

- (1) 24" STOP BAR
- (2) 6" WHITE LINE
- (3) CROSSWALK PER MUTCD STANDARDS



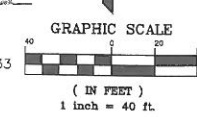
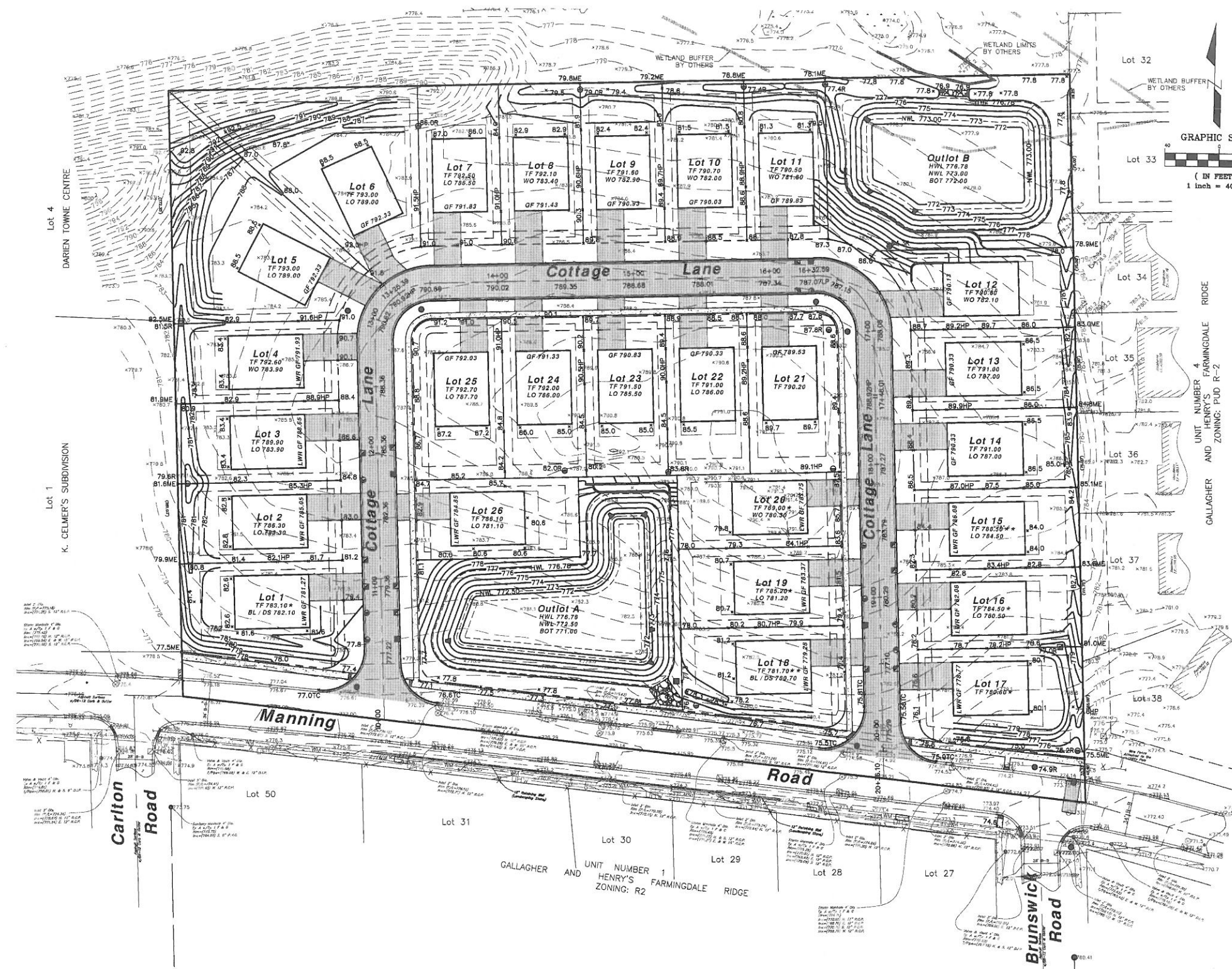
NO.	DATE	DESCRIPTION	BY
1	04-24-15	PER COUNTY & VILLAGE REVIEW	ADM
2	08-24-15	PER VILLAGE REVIEW	ADM

2100 MANNING ROAD, LLC
 401 E PROSPECT AVENUE
 MT. PROSPECT, ILLINOIS 60056
 847-670-6710

FINAL ENGINEERING PLANS
 FOR
 MANNING WOODS
 DARIEN, ILLINOIS

DESIGNTEK ENGINEERING, INC.
 CONSULTING AND SITE DESIGN ENGINEERS
 9930 W. 190TH STREET, SUITE L
 MOKENA, ILLINOIS 60448
 (708) 326-4961
 IL PROF. LIC. NO.: 184-003740

PROJECT INFORMATION	
Project No.:	14-0056
Scale:	1" = 40'
Date:	02-13-15
Design By:	ADM
Drafted By:	ADM
Checked By:	SOS



GRADING/PAD NOTES

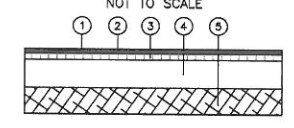


- TF XXX.XX TOP OF FOUNDATION ELEVATION
- LO XXX.XX LOOKOUT ELEVATION
- WO XXX.XX WALKOUT/BSMT FLOOR @ WALKOUT ELEVATION (TYPICAL: WO = TF - 8.67')
- BL XXX.XX OR DS XXX.XX DENOTES "BRICK LEDGE" OR "DROP SIDING"
- SHADED DRIVE DENOTES RESTRICTED GARAGE LOCATION NECESSARY FOR 8% MAX. SLOPE
- GF XXX.XX INDICATES GARAGE FLOOR ELEVATION (TYPICAL: GF = TF - 8')
- LWR GF XXX.XX INDICATES A LOWERED GARAGE FLOOR ELEVATION IS REQUIRED TO MAINTAIN 8% MAX. DRIVEWAY SLOPE. (ONE ADDITIONAL STEP IN GARAGE) (TYPICAL: LWR GF = TF - 15')
- TF XXX.XX* & LWR GF XXX.XX INDICATES A LOWERED GARAGE FLOOR ELEVATION IS REQUIRED TO MAINTAIN 8% MAX. DRIVEWAY SLOPE. (TWO ADDITIONAL STEPS IN GARAGE) (TYPICAL: LWR GF = TF - 22')
- TF XXX.XX** & LWR GF XXX.XX INDICATES A LOWERED GARAGE FLOOR ELEVATION IS REQUIRED TO MAINTAIN 8% MAX. DRIVEWAY SLOPE. (THREE ADDITIONAL STEPS IN GARAGE) (TYPICAL: LWR GF = TF - 29')

GRADING CONVERSIONS

- EDGE OF PAVEMENT (EP) = CL - 0.25'
- FLOW LINE (FL) = CL - 0.33'
- TOP OF CURB (TC) = CL + 0.16'
- RIGHT OF WAY W/WALK = CL + 0.55'
- RIGHT OF WAY W/O WALK = CL + 0.59'

PAVEMENT SECTION



1. 1.5" HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50
2. PRIME COAT, MC-30, AT 0.15 GAL./S.Y.
3. 4" HOT MIX ASPHALT BINDER COURSE, N50
4. 6" AGGREGATE BASE COURSE, TYPE B, CA-6, COMPACTED W/ NO SLAG
5. WELL DRAINED SUB-GRADE (SUB-GRADE TO MODIFIED 95 PROCTOR MIN. COMPACTION)

NO.	DATE	DESCRIPTION	BY
1	04-24-15	PER COUNTY & VILLAGE REVIEW	ADM
2	08-24-15	PER VILLAGE REVIEW	ADM

2100 MANNING ROAD, LLC
 401 E PROSPECT AVENUE
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Design By:	ADM
Drafted By:	ADM
Checked By:	SDS

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AGENDA MEMO
MUNICIPAL SERVICES COMMITTEE
MEETING DATE: July 27, 2015

Issue Statement

PZC 2015-06: 951 N. Frontage Road, WoodSpring Suites (formerly Value Place Hotel):

Petitioner seeks approval of the following:

1. To rezone subject property from OR&I Office, Research and Light Industry to B- 3 General Business, Zoning Ordinance Section 5A-6-2.
2. Special use for a hotel within the B-3 zoning district, Zoning Ordinance Section 5A-8-4-4.
3. Variation to increase the maximum permitted building from 40 feet to 50 feet, Zoning Ordinance Section 5A-8-4-9.
4. Variation to decrease the minimum required foundation landscaping with from 10 feet to 7 feet, Zoning Ordinance Section 5A-10-7(A).
5. Variation to increase the maximum permitted free-standing sign height from 12 feet to 30 feet, Sign Code Section 4-3-10(B)(3).
6. Variation to permit a pole sign, Sign Code Section 4-3-7(B)(1);
7. Variation to permit an electronic message board sign, Sign Code Section 4-3-10(B)(3).
8. Variation to permit wall signage on the east and west building facades which do not face either a public right-of-way or building frontage, Sign Code Section 4-3-10(B)(2).

General Information

Petitioner:	Holladay Properties 6370 Ameriplex Drive, Suite 110 Portage, IN 46368
Property Owner:	Endlichhofer Trust 1S2780 Summit Court, C-2 Oakbrook Terrace, IL 60181
Property Location:	951 N. Frontage Road
PIN:	09-34-302-019
Existing Zoning:	OR&I Office, Research and Light Industry
Existing Land Use:	Vacant
Proposed Zoning:	B-3 General Business
Proposed Land Use:	Hotel
Comprehensive Plan Update:	Office
Surrounding Zoning and Land Use:	

North: R-3 Multi-Family Residence: townhomes (single-family attached homes)
East: B-1 Local Business (DuPage County): single-family residence
South: R-1 Single-Family Residence and I-1 General Industrial: I-55, City's Municipal Services Facility on south side of I-55.
West: B-1 Local Business (DuPage County): landscape

Size of Property: 3.4 acres
Floodplain: None.
Natural Features: Property has several small wetland areas.
Transportation: Property has access onto N. Frontage Road
History: Planning and Zoning Commission reviewed a concept plan in December 2014.

Documents Submitted

This report is based on the following information submitted to the Community Development Department by the Petitioner:

1. Plans, includes ALTA Survey, site plan, engineering, landscape, building elevations and photometric plans, 18 sheets, prepared by Holladay Properties, Nelson Surveyors, Inc., and CivWorks Consulting, LLC., submittal date May 18, 2015.
2. Sign Survey, 7 sheets, prepared by AGI, dated May 23, 2015.
3. Traffic Study, prepared by KLOA, dated March 10, 2015

Planning Overview/Discussion

The subject property is located on the south side of N. Frontage Road at Mystic Trace.

The petitioner proposes a 4-story, 42,604 square foot, 124 room extended stay type hotel.

Rezoning

In order to accommodate the proposed use, the property needs to be rezoned from OR&I Office, Research and Light Industry to B-3 General Business. Staff does not object to the proposed zoning classification.

Special Use

Within the B-3 zoning district, a hotel requires special use approval. Special uses are those uses specifically listed as such, that is, the use of the property requires City Council approval. Staff does not object to the proposed special use.

Site Plan

Generally, staff finds the site plan is laid out well. Building and parking setbacks are met, building floor area ratio and lot coverage comply. The trash enclosure is located on the south side of the property, screened by CMU block masonry enclosure.

The number of parking spaces provided complies, 1 space per room plus 1 space per employee required and provided. The number of handicapped spaces provided complies. The site plan shows a total of 129 parking spaces, which includes 5 handicapped accessible spaces. The parking stall dimensions as well as driveway aisles and maneuvering areas comply.

Driveway location and dimensions comply with the Darien Zoning Ordinance. However, N. Frontage Road (Joliet Road) and I-55 are the State's jurisdiction. Therefore, work within these right-of-ways requires a permit from the Illinois Department of Transportation (IDOT). Among the comments from IDOT, the driveway entrance is to be moved so it is opposite Mystic Trace. Due to the wetlands in that portion of the property, coordination with DuPage County will be needed.

Landscaping

The landscape plan shows plantings along the perimeter of the site (areas outside of the wetlands at the northwest area of the parcel), within parking lot landscape islands, around the building foundation and along the north edge of the parking lot. Generally, the landscape plan meets the intent of the Zoning Ordinance.

Perimeter yard landscaping is lacking along the north and west side of the parcel. However, the northwest corner of the parcel is occupied by wetlands which are shown to remain. The Zoning Ordinance allows for existing vegetation and other landscape features which are to be preserved to meet the landscape requirements. The petitioner should confirm how the wetlands will be maintained and/or enhanced.

Within parking lot landscape islands, additional shrubs are required. However, if the landscape planner believes the proposed planting schedule will fill the landscape island with plant material, then this item can be addressed at the time of final inspection.

The petitioner is seeking a variation for a building foundation landscape strip less than 10 feet wide (shown to be between 7-8 feet), and parking landscape islands have less than the required amount of landscaping. The petitioner should explain the need for this variation. The plan shows landscaping along the building foundation, combination of trees and shrubs.

Building Elevations

The exterior building materials include: face brick, Hardiplank lap siding and trim, glass and aluminum doors, plastic shutters, composition shingles.

The petitioner is requesting a variation from the Zoning Ordinance to permit a 4-story, 50-foot tall building where 3-stories not to exceed 40 feet is permitted. Given the site's location along I-55

and the proposed use, staff does not object to this variation. The building has been pushed away from N. Frontage Road, the building is over 140 feet from N. Frontage Road.

Signage

The plan proposes two free standing signs, a ground sign located along N. Frontage Road and a pole or pylon sign located along I-55. The pole sign includes an electronic message board. Wall signage is proposed on the north, east and west sides of the building.

The ground sign and wall sign on the front façade of the building comply in terms of location, size and height.

The petitioner is seeking variations from the Sign Code for the pole sign, pole sign height, electronic message board and wall signage on the east and west building facades. Given this site's location along I-55, the need for motorists on I-55 to be able to see and find the hotel site, staff does not object to this variation.

Photometric

The exterior lighting plan shows the amount of light at the property line complies. Regardless, once constructed, light shields will be required if there is light glare onto an adjacent property or road right-of-way.

Traffic Study

The petitioner submitted a traffic study conducted by Kenig, Lindgren, O'Hara, Aboona, INC (KLOA) dated March 10, 2015. The traffic study concludes:

- The development will generate a low volume of traffic, as a result, will not have a significant impact on area roadways.
- The existing roadway system is sufficient to accommodate the traffic to be generated by the development.
- The Cass Avenue and N. Frontage Road intersection is and will continue operating at acceptable levels of service.
- N. Frontage Road westbound maximum queues currently experienced at the Cass Avenue intersection will not be exacerbated by the proposed development.
- The proposed single access driveway in/out of the development will be able to sufficiently accommodate all development traffic.

The traffic study does not recommend any improvements or alterations to adjacent roadways.

Engineering/Stormwater Management/Wetlands

The property contains several small wetlands. Wetlands fall under DuPage County's jurisdiction. The petitioner has submitted plans to the County for review. The County must certify plans comply with the Countywide Stormwater Management Ordinance before any construction permits can be issued by the City. The petitioner has made their submittal to DuPage County.

A combination of basins and underground storage vaults will provide stormwater detention. City The City Engineer provided comments in a letter dated June 23, 2015, from Christopher B. Burke Engineering. The comments are minor.

Zoning Ordinance Variation Criteria

1. Whether the general character of the property will be adversely altered.
2. Whether the overall value of the property will be improved and there will not be any potential adverse effects on the neighboring properties.
3. Whether the alleged need for the variation has been created by any person presently having a proprietary interest in the premises.
4. Whether the proposed variation will impair an adequate supply of light and air in adjacent property, substantially increase congestion in the public streets, increase the danger of fire or endanger the public safety.
5. Whether the proposed variation will adversely alter the essential character of the neighborhood.

Sign Code Variation Criteria

1. To promote and protect the public health, safety, comfort, morals, convenience and general welfare of the residents of the City.
2. To enhance the physical appearance of the City by preserving the scenic and natural beauty of the area.
3. To promote the safety and recreational value of public travel.
4. To protect the public investment in streets and highways by reducing sign or advertising distractions that may increase traffic accidents.
5. To ensure compatibility of signs with surrounding land uses.
6. To enhance the economy of the City by promoting the reasonable, orderly and effective display of outdoor advertising.
7. To protect the pedestrians and motorists within the City from damage or injury caused by distractions, obstructions and hazards created by a proliferation of off-site advertising signs.
8. To prevent the proliferation of off-site advertising signs which distract from the development of the City in an aesthetically pleasing manner.
9. To preserve the character of the City which is a single-family residential community by assuring the compatibility of signs with the surrounding land uses.

Staff Findings/Recommendations

The requested variations from the Zoning Ordinance will not will impair an adequate supply of light and air in adjacent property, will not substantially increase congestion in the public streets, will not increase the danger of fire or endanger the public safety.

The requested variations from the Sign Code will enhance the economy of the City by promoting reasonable, orderly and effective display of outdoor advertising.

Therefore, staff recommends the Planning and Zoning Commission make the following recommendation approving the petition subject to conditions:

Based upon the submitted petition and the information presented, the request associated with PZC 2015-06 is in conformance with the standards of the Darien City Code and, therefore, I move the Planning and Zoning Commission approve the petition as presented, subject to the following conditions:

1. Address comments noted in **letter dated June 23, 2015**, from Dan Lynch, PE, Christopher B. Burke Engineering, Ltd.
2. Parking landscape islands to be full of plant material, final inspection to verify.
3. All exterior lighting to be directed in a way to avoid causing glare onto adjacent properties. The need for light shields to be verified during final inspection.

Planning and Zoning Commission Review – July 1, 2015

The Planning and Zoning Commission considered this matter at its meeting on July 1, 2015. The following members were present: Beverly Meyer – Chairperson, Andrew Kelly, John Laratta, John Lind, Raymond Mielkus, Kenneth Ritzert, Michael Griffith – Senior Planner.

Absent: Ronald Kiefer, Louis Mallers, Pauline Oberland, Elizabeth Lahey – Secretary.

Michael Griffith, Senior Planner, reviewed the agenda memo. He noted the zoning requests being made, including rezoning, special use and variations. He noted there are wetlands on the property which the petitioner is attempting to avoid, noted the City Engineer has reviewed the plans and his comments are minor, noted IDOT has jurisdiction on the adjacent road right-of-ways and wants the driveway moved to be opposite Mystic Trace. He noted a traffic study was done, noting the study did not indicate any issues with the proposed use nor recommend any improvements to the adjacent roadway.

Drew Mitchell, VP of Development, Holladay Properties, the petitioner, went over the site plan. He stated the hotel will have 124 rooms, it will be an extended stay type hotel. He stated the proposed building façade has more brick than other WoodSpring hotel locations. He stated they are working to address all comments, including from the City Engineer, DuPage County and IDOT.

Chairperson Meyer asked if handicapped parking stalls would be provided and if there was a photometric plan. She asked if an emergency call box could be installed in the parking lot given the proximity to I-55.

Mr. Mitchell stated they have the required amount of handicapped parking stalls and the photometric plan was included in the plans submitted and complies. He stated they do not object to installing an emergency phone box.

Commissioner Kelly asked if this would be a WoodSprings Signature hotel, he asked about the frequency of room cleaning.

Ryan Kelly, VP of Planning and Design, Holladay Properties, stated the Signature brand is only a concept at this time. He stated the rooms are cleaned bi-weekly. He stated they offer al-a-cart services so guests pay for only the services they want or need.

Commissioner Laratta asked about their construction time frame.

Mr. Mitchell stated they hope to begin construction in September, taking approximately 7 months to complete.

Commissioner Kelly asked about their target customer.

Mr. Mitchell stated their customers are blue/gray collar business travelers who need a place to stay for a week or two while they are in the Chicago area, who are budget conscience. He stated they also displaced families and travelers who need a place to stay for a week or longer such as those visiting family in the area for an extended period of time.

Commissioner Kelly asked how long a person can stay at the hotel.

Mr. Mitchell stated there is not a specific time period but the hotel is not setup for long term stays, stating people cannot receive mail at the hotel, for example.

Commissioner Laratta asked if food and alcohol service is provided.

Mr. Mitchell stated no.

The exterior lighting plan was discussed, noting where the lights are to be located. Mr. Mitchell noted the lighting plan is intended to keep light on the property and not spill over onto adjacent properties.

Commissioner Ritzert asked if they did soil borings to determine if the site can support structure. He asked what was being done with stormwater.

Mr. Mitchell stated yes, stating the soil reports indicates the site can support the structure.

Oswaldo Pastrana, CivWorks Consulting, LLC, petitioner's engineer, described the stormwater management plan, noting the detention basins and underground storage vaults.

Commissioner Ritzert asked if there would be fire walls. He asked about the exterior building materials.

Mr. Mitchell stated there will be fire walls, that the building plans will comply with all applicable building and fire codes. He stated the building would have fire sprinklers. He provided the material board, noting Hardiplank and face brick. He described the exterior building elevations.

Commissioner Lind said he was concerned with the materials. He said he would like to see more masonry and less Hardiplank, the I-55 side of the building should be dressed up more since it is the City's front door. He said the dumpster enclosure should be 8-9 feet tall to better screen the dumpsters given the property is lower than I-55.

The Commission discussed exterior building materials. Mr. Mitchell stated they increased the amount of brick for this location over other WoodSpring sites at the request of City staff. He stated the proposed building has 45% brick. Mr. Mitchell stated Hardiplank is a durable product, not aluminum or vinyl siding.

The Commission agreed the amount of brick should be bumped up to 55-60%, with a goal of 60%, with emphasis on the I-55 side. Mr. Mitchell stated he thought was achievable.

Carolyn Vena, Board Member, Violets Farm Townhomes, stated she is pleased something is happening to this property, but stated she is not in favor of the 30-foot tall sign with the message board. She stated they prefer the driveway to be on the east side of the property and not opposite Mystic Trace to discourage people driving through the neighborhood to the north. She stated her concern with the building height, stating it should be limited to 40 feet.

Richard Critez, Violets Farm, stated he prefers a lower building, lower light levels, lower sign, does not see the need for signs on the side of the building, more canopy trees.

It was noted the 30-foot sign is located along I-55 and the sign is oriented to be seen from I-55, that the side of the sign maybe visible from Frontage Road. Mr. Mitchell stated the electronic message board will have static messages, displaying room rates for example. He stated it is a small portion of the sign.

Kurt Drescher, stated he owns the property to the east, Drescher Landscaping. He stated he is not comfortable with a hotel on the property. He questioned the proposed B-3 zoning, stating there is not property zoned B-3 nearby. He questioned where there is a building of similar height. He said the hotel will change the character of the area. He said he is concerned with safety, he has had equipment stolen from his business next door. He asked about the drainage ditch along Frontage Road, stating the area where the driveway is to be located floods. He stated the wetlands on the property do not look good and would like something done to improve their appearance.

Mr. Pastrana explained how the ditch along Frontage Road will be improved through regrading and a culvert. He explained the County instructed them to avoid the wetlands as much as possible which dictated the building location as well as the driveway location.

Mr. Mitchell stated having a business on the property would help to deter crime.

Judy Gruman, Trustee for the property to the west at 17w615 N. Frontage Road, questioned where the stormwater will flow noting her property floods.

Mr. Pastrana stated water flows from west to east, stating water coming onto her property mostly comes to the west of her property but noted that water from the wetlands on the subject property also comes onto her property. He stated the proposed improvements to the road side ditch should help.

Without further discussion, Commissioner Laratta made the following motion, seconded by Commissioner Mielkus:

Based upon the submitted petition and the information presented, the request associated with PZC 2015-06 is in conformance with the standards of the Darien City Code and, therefore, I move the Planning and Zoning Commission approve the petition as presented, subject to the following conditions:

- 1. Address comments noted in letter dated June 23, 2015, from Dan Lynch, PE, Christopher B. Burke Engineering, Ltd.**
- 2. Parking landscape islands to be full of plant material, final inspection to verify.**
- 3. All exterior lighting to be directed in a way to avoid causing glare onto adjacent properties. The need for light shields to be verified during final inspection.**
- 4. Exterior building material to be 55-60% clay, face brick, with petitioner to aim for 60%, emphasis given to I-55 side of building.**

**Upon a roll call vote, THE MOTION CARRIED by a vote of 6-0.
(Absent: Commissioners Kiefer, Mallers and Oberland.)**

Municipal Services Committee – July 27, 2015

Since the Commission meeting, staff has been coordinating efforts with the petitioner, IDOT and DuPage County regarding relocating the driveway opposite Mystic Trace, to address IDOT requirements. The location required by IDOT will further impact wetlands which fall under the County's jurisdiction. Staff does not object to the driveway location opposite Mystic Trace.

Also, staff has coordinated efforts to see if the wetlands can be enhanced to address comments made at the public hearing. Again, wetlands fall under the County's jurisdiction.

The plans presented to the Committee show the driveway located on the east end of the property and not show enhancements to the wetlands.

All comments, including engineering, stormwater, IDOT and County, will have to be addressed prior to the Council's consideration.

Based on the Commission's recommendation, staff recommends the Committee make the following motion:

Based upon the submitted petition and the information presented, the request associated with PZC 2015-06 is in conformance with the standards of the Darien City Code and, therefore, I move the Municipal Services Committee approve the petition as presented, subject to the following conditions:

- 1. Address comments noted in letter dated June 23, 2015, from Dan Lynch, PE, Christopher B. Burke Engineering, Ltd.**
- 2. Parking landscape islands to be full of plant material, final inspection to verify.**
- 3. All exterior lighting to be directed in a way to avoid causing glare onto adjacent properties. The need for light shields to be verified during final inspection.**
- 4. Exterior building material to be 55-60% clay, face brick, with petitioner to aim for 60%, emphasis given to I-55 side of building.**
- 5. Revised site plan (driveway location) and landscape (wetland enhancements) subject to IDOT and Dupage County approval.**

Decision Mode

Planning and Zoning Commission: July 1, 2015

Municipal Services Committee: July 27, 2015



CHRISTOPHER B. BURKE ENGINEERING, LTD.

9575 West Higgins Road Suite 600 Rosemont, Illinois 60018 TEL (847) 823-0500 FAX (847) 823-0520

June 23, 2015

City of Darien
1702 Plainfield Road
Darien, IL 60561

Attention: Mike Griffith

Subject: Value Place Hotel – Final Site Development Plans
(CBBEL Project No. 950323H186)

Dear Mike:

As requested, we have reviewed the Final Site Development Plans and supporting documents for the aforementioned property. The following comments are submitted for your consideration:

FINAL SITE DEVELOPMENT PLANS

The following comments pertain to the Final Site Development Plans prepared by Civil Works Consulting, LLC and dated May 12, 2015.

Sheet C0.0 – Cover Sheet

No Comments.

Sheet C1.0 – Existing Conditions

1. Provide documentation that the survey complies with the datum criteria outlined in Section 15-33 of the DuPage County Stormwater Ordinance.

Sheet C2.0 – Site Demolition Plan

1. Impact to the existing wetlands will have to be reviewed and certified by DuPage County. We understand it is under review.

Sheet C3.0 – Site Dimensional and Paving Plan

No Comments

Sheet C4.0 - Site Utility Plan

1. The proposed sanitary sewer is crossing less than six (6") inches below the existing 20" watermain. Please show the proposed auger pit locations as it appears the proposed auger pit would impact the existing watermain. Consider moving the auger

pit south and auger the sanitary sewer beneath the existing watermain. If the casing pipe extends to 10 feet south of the watermain, that would meet the protection standards. If another method is used, it must be specified on the plan.

2. Add a note the Frames and Grates Specifications that all storm grates will have a "Drains to Creeks" emblem.

Sheet C5.0 – Site Grading and Erosion Control Plan

No Comments.

Sheet C5.1 – Soil Erosion and Sediment Control Details

No Comments.

Sheet C6.0, C6.1, C6.2 Construction Details

1. Revise note 2 of the pipe bedding detail to ductile iron pipe, not PVC for watermain.

Sheet C7.0 General Conditions and Detailed Specifications

1. All water valves shall be resilient wedge type.

Sheet L1.0 – Site Landscaping Plan

No Comments.

Sheet L2.0 – Landscape Details and Notes

Not Reviewed.

Sheet P1.0 – Site Photometrics Plan

No comments.

Sheet P2.0 – Site Photometrics Plan

No comments.

General Comments

1. The sanitary sewer shall be reviewed and approved by the DuPage County Public Works.
2. An IEPA NPDES permit is require and the SWPPP must be submitted to the City.
3. An engineer's estimate must be submitted to establish the letter of credit as required in the City Stormwater Ordinance.
4. A permit is required from IDOT for work within the IDOT right of way.

STORM WATER MANAGEMENT REPORT

The revised pages must be incorporated into the Stormwater Submittal, and a complete tabular submittal provided for the file.

Exhibit No. 2.2 and 2.4:

No Comments.

TR55 Tc Worksheet – Hyd. No. 9

No Comments

Curve Number Calculations

1. Subarea 4B and 5A areas do not match the exhibit areas. Please revise.

Storm Trap Details

1. Add geotextile fabric to separate the aggregate stone and soil to minimize migration of soil into the infiltration system.
2. Provide mastic for the frames and adjusting rings.
3. The plans must be signed and sealed by a Registered Professional Engineer or Structural Engineer.

General Comments

1. Review will be required by DuPage County for impacts and mitigation to the wetland. The tabs in the stormwater submittal should be numbered as detailed in the flow chart found on the County website. References in the following comments that reference a tab, refer to the numbered tab from the flow chart.
2. Provide the signed statement by the land owner and developer as required in Section 15-47.A.3.

If you have any questions, please feel free to contact me.

Sincerely,



Daniel L. Lynch, PE, CFM
Head, Municipal Engineering Department

Cc: Dan Gombac – City of Darien
Osvaldo Pastrana
Civil Works Consulting, LLC
3343 N. Neva Avenue
Chicago, IL 60634



Illinois Department of Transportation

Division of Highways/Region One / District One
201 West Center Court/Schaumburg, Illinois 60196-1096

PERMITS

Location: Joliet Rd. at Mystic Terrace
Municipality: City of Darien, DuPage Co.
Reference No: 022-65990
Re: Value Place Hotel

June 18, 2015

Mr. Osvaldo Pastrana
CivWorks Consulting, LLC
3343 N. Neva Ave.
Chicago, IL. 60634

Dear Mr. Pastrana:

We have completed our review of your submitted engineering drawings for the subject location. Our comments are listed below.

1. The Department does not concur with the location of the proposed commercial access driveway to Joliet Rd. It appears that the proposed access driveway is very near a low point along the Highway drainage ditch line. Please relocate the commercial access driveway to align with the center line of Mystic Terrace to provide better interaction with the State highway.
2. Please provide all the information required on the attached "Drainage Connection Check List" for your drainage connection to I-55 and Joliet Rd.
3. We noticed that there is quite a bit of standing water along the frontage of your site. Please regrade the drainage ditch to provide positive drainage flow within the State right of way. Also, label the elevation of the bottom of the drainage ditch and provide flow arrows on your plans and cross sections.
4. Provide an existing/proposed driveway profile and a driveway detail in your plan set. Please revise the width of the driveway to 24' (E-E Min).
5. Delete the stop sign and stop bar at the driveway entrance to Joliet Rd, as traffic signs are not required for this particular location.
6. Please insure that the monument sign will not obstruct the motoring public utilizing the proposed commercial access driveway to Joliet Rd.
7. Provide a plan/profile for the proposed sanitary sewer to be installed under Joliet Rd. and note all structures involved. Show/dimension the receiving/auguring pits on your plans.
8. Show the topography/impacts and restoration on the north side of the frontage Rd. where the sanitary sewer is to be connected on your plans.

City of Darien
Received
JUN 24 2015
Community Development

Miles
FYI - PLU

Location: Joliet Rd. at Mystic Terrace

Date: June 18, 2015

Page 2

9. Please label the State right of way property lines and the edge of pavement on your plans

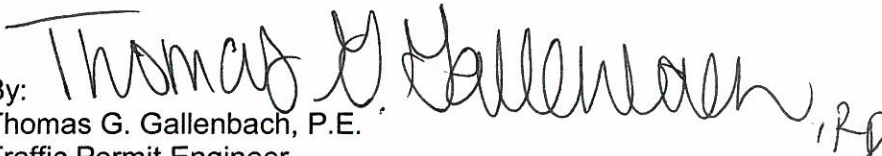
10. Include IDOT Standard Lane/shoulder closure details applicable to the work within the State right of way in your plan set.

Please revise your drawings/plans in accordance with the above comments and resubmit two (2) copies with written disposition of comments to continue the review process.

If you have any questions regarding this matter, please contact Mike Wisniewski at (847) 705-4541.

Very truly yours,

John A. Fortmann, P.E.
Deputy Director of Highways
Region One Engineer

By: 
Thomas G. Gallenbach, P.E.
Traffic Permit Engineer

Cc: Daniel Gombac – City of Darien

MEMORANDUM TO: Osvaldo Pastrana, P.E., LEED AP
CivWorks Consulting, LLC

FROM: Javier Millan
Senior Consultant

Michael Werthmann, PE, PTOE
Principal

DATE: March, 10, 2015

SUBJECT: Traffic Impact Study
Proposed Hotel Development
Darien, Illinois

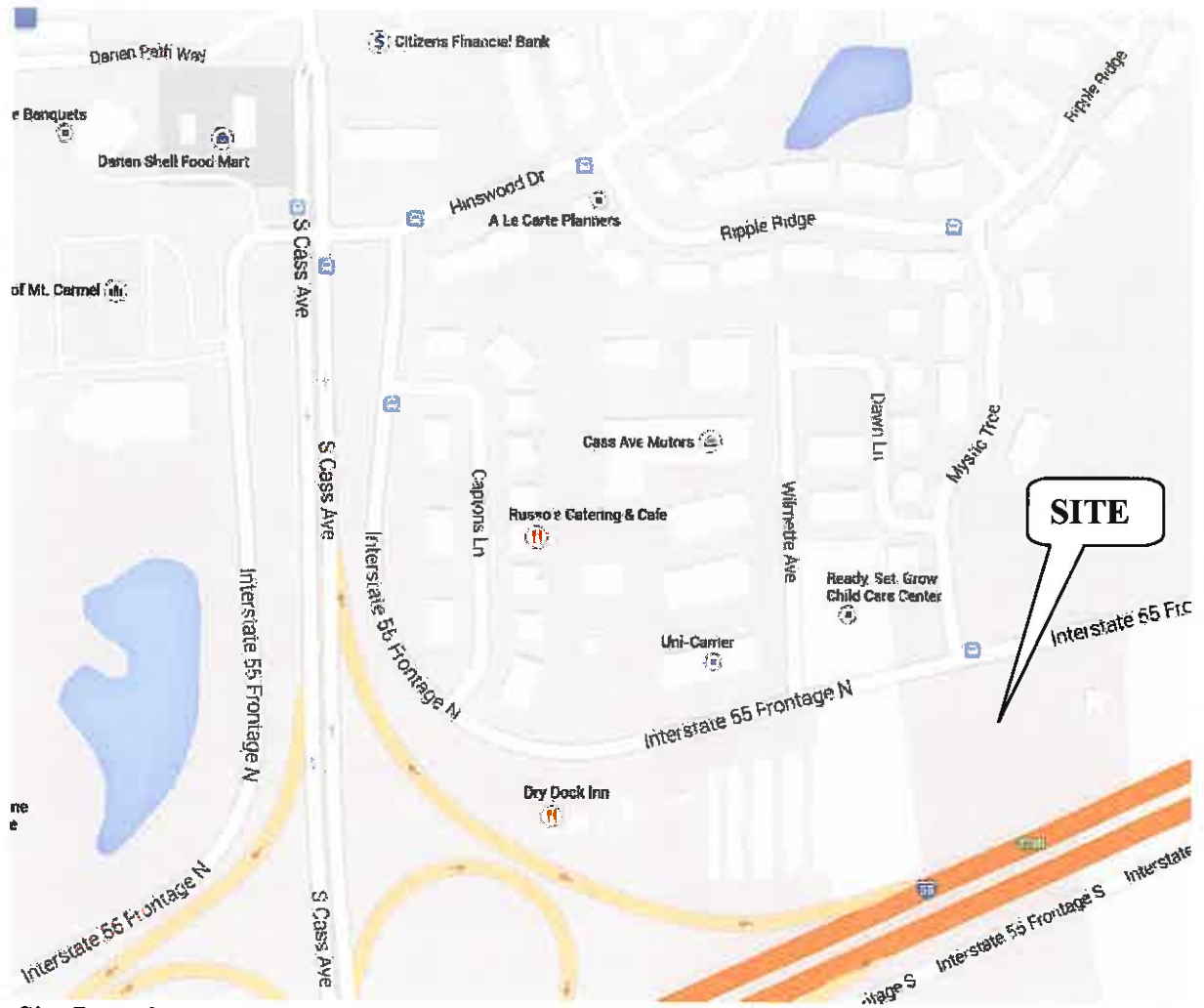
This memorandum summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed hotel to be located at 951 North Frontage Road in Darien, Illinois. The plan calls for a 124-room hotel with 129 off-street parking spaces. Access will be provided via a single full-ingress/egress access drive on North Frontage Road.

Figure 1 shows the location of the development in relation to the area roadway system and **Figure 2** shows an aerial view of the site.

The purpose of this study was to examine background traffic conditions, assess the impact that the proposed development will have on traffic conditions in the area, and determine if any roadway improvements are necessary to accommodate traffic generated by the proposed development.

The sections of this memorandum present the following.

- Existing conditions of the study area
- A description of the proposed development
- Directional distribution of the development traffic
- Vehicle trip generation for the development
- Traffic analyses for the weekday morning and evening peak hours
- Recommendations with respect to adequacy of the development access and the adjacent roadway network



Site Location

Figure 1



Aerial View of Site Location

Figure 2

Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future traffic conditions. The following provides a summary of the physical characteristics of the area roadways including geometry, traffic control, and existing peak hour traffic volumes.

Site Location

The site is bounded by I-55 (Stevenson Expressway) to the south and the I-55 North Frontage Road to the north. Access to and from I-55 is provided to the southwest of the site via a full interchange with Cass Avenue. The frontage road provides access to multiple residential developments and businesses in the area.

Existing Roadway System Characteristics

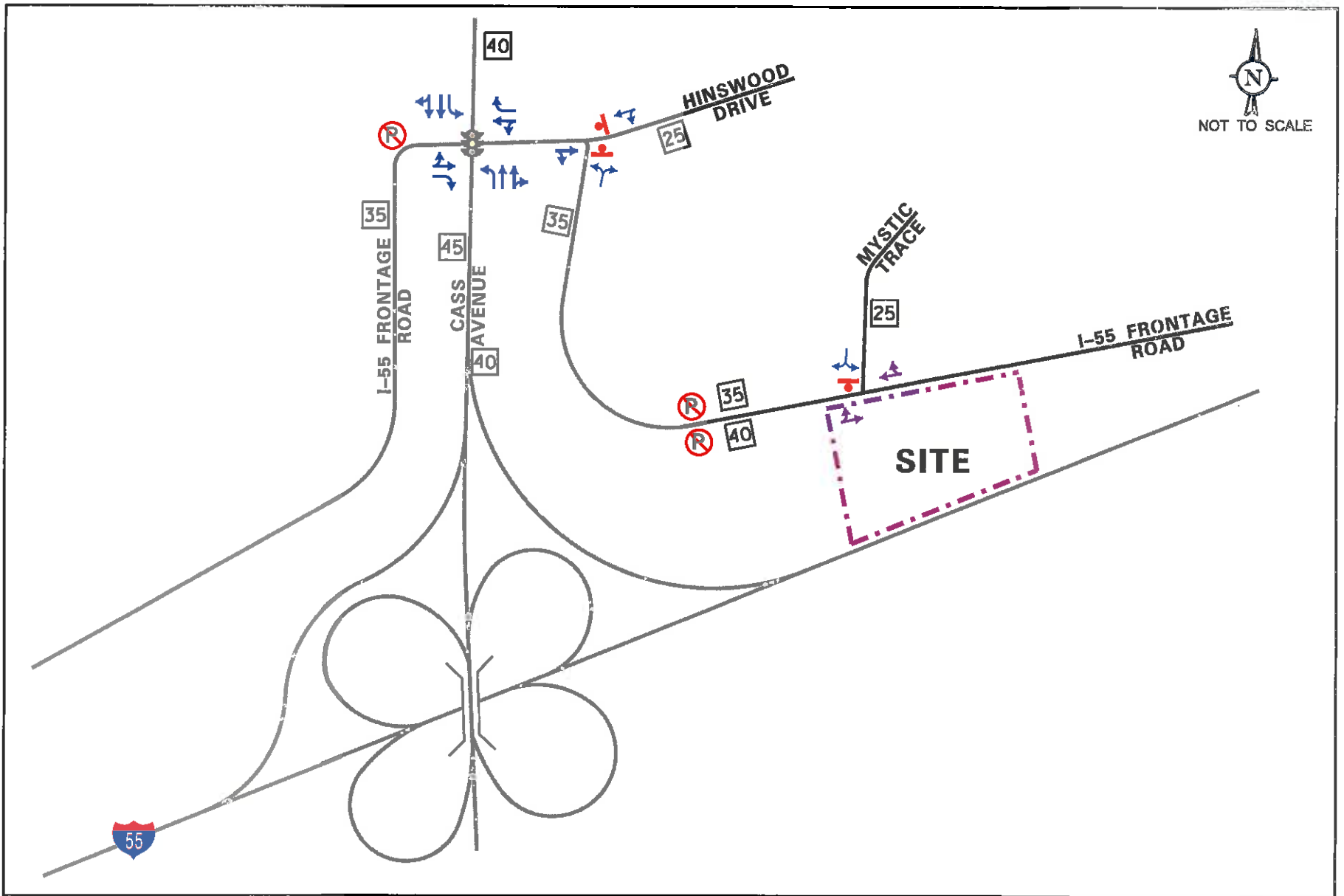
The characteristics of the existing roads within the study area are described below and illustrated in **Figure 3**.

Cass Avenue is a north-south arterial roadway that generally has two lanes in each direction. At its signalized intersection with I-55 Frontage Road/Hinswood Drive, Cass Avenue provides one exclusive left-turn lane, one through lane, and one combined through/right-turn lane on each approach. Northbound and southbound Cass Avenue provides interchange ramps for all directions onto I-55. Cass Avenue has an Average Daily Traffic (ADT) volume of 23,200 vehicles, has a posted speed limit of 40 mph, and is under the jurisdiction of the DuPage County Division of Transportation (DuDOT).

I-55 North Frontage Road is a two-lane road that provides local access along I-55 from Lemont Road to the west to 79th Street to the east. The posted speed limit is 35 mph increasing to 40 mph along the frontage of the site. On-street parking is prohibited on both sides of the road. No exclusive turn lanes are provided at its unsignalized intersections with Mystic Trace and Hinswood Drive. The three-way intersection of the frontage road with Hinswood Drive is under two-way stop sign control, with stop signs provided for the east and south approaches. The frontage road has an ADT of 3,000 vehicles and is under the jurisdiction of IDOT.

Hinswood Drive is an east-west two-lane local road that provides access to numerous multi-family residential developments. At its unsignalized intersection with the frontage road, Hinswood Drive is under stop sign control and provides a combined left/through lane. Hinswood Drive is under the jurisdiction of the City of Darien and has a posted speed limit of 25 mph.

Mystic Trace is a north-south, two-lane local road that extends from the frontage road north to its terminus at Ripple Ridge. The road has a posted speed limit of 25 mph and is under the jurisdiction of the City of Darien. At its intersection with the frontage road, Mystic Trace has a one-lane approach that is under stop sign control.



PROJECT:
 I-55 Frontage Road
 Hotel Development
 Darien, Illinois

TITLE:
 Existing Street Characteristics

KLOA
 Job No: 15-046
 Figure: 3

Existing Traffic Volumes

In order to determine current traffic conditions within the study area, KLOA, Inc. conducted peak period traffic counts at the following intersections.

- Cass Avenue and I-55 North Frontage Road/Hinswood Drive
- I-55 North Frontage Road and Hinswood Drive
- I-55 North Frontage Road and Mystic Trace

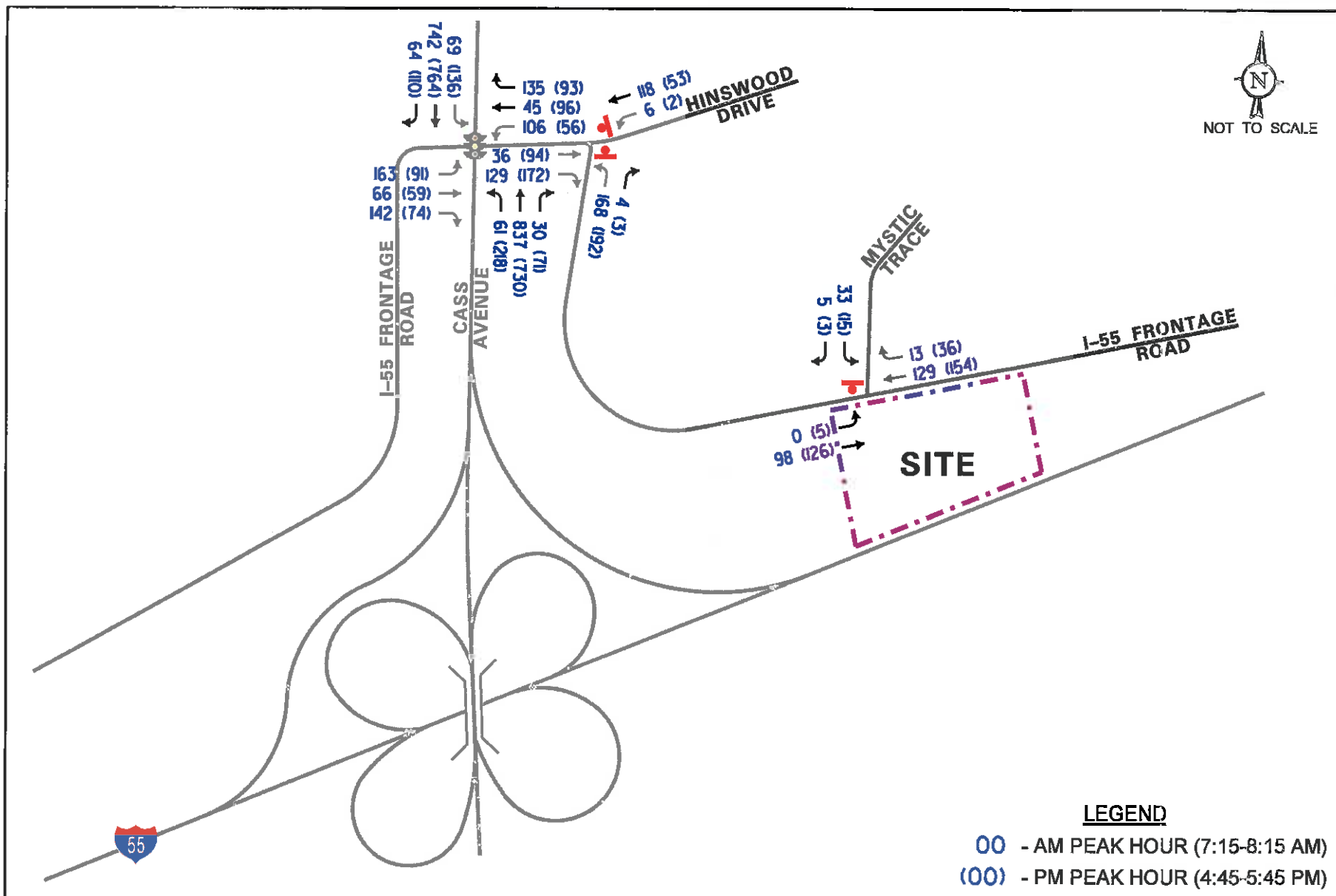
The traffic counts were conducted on Tuesday, February 24, 2015 during the morning (7:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods. Summary of the traffic count data indicated that the peak hour of traffic occurs between 7:15 A.M. and 8:15 A.M. during the morning peak hour and 4:45 P.M. to 5:45 P.M. during the evening peak hour. **Figure 4** illustrates the existing peak hour traffic volumes. The turning movement count volumes are included in the Appendix.

Existing Traffic Observations

In addition to the traffic counts, KLOA, Inc. also observed traffic conditions during the morning and evening peak hours, particularly the westbound queues on the frontage road at its signalized intersection with Cass Avenue and how it impacts the intersection of the frontage road with Hinswood Drive. The following is a summary of our observations:

Morning Peak Period

- Westbound queues on the frontage road at its intersection with Cass Avenue were typically two to three vehicles and were contained within the approximately 90 feet of storage provided between Cass Avenue and Hinswood Drive.
- On a few occasions, the westbound queues extended beyond the provided storage area with one vehicle waiting on the east approach (Hinswood Drive) and another vehicle waiting on the south approach (frontage road). All of these vehicles cleared within the green phase of the traffic signal.
- There was one time (8:22 A.M.) in which the westbound queues extended beyond the provided storage area and it was observed that three vehicles were queued on Hinswood Drive and on the frontage road. All of these vehicles cleared within the green phase of the traffic signal.
- Northbound queues on the frontage road at its intersection with Hinswood Drive were mostly one to two vehicles with a maximum queue of four vehicles occurring only once.



LEGEND
 00 - AM PEAK HOUR (7:15-8:15 AM)
 (00) - PM PEAK HOUR (4:45-5:45 PM)

PROJECT:
 I-55 Frontage Road
 Hotel Development
 Darien, Illinois

TITLE:
 Existing Traffic Volumes

KLOA
 Job No: 15-048
 Figure: 4

Evening Peak Period

- On four occasions, westbound queues extended beyond the provided storage area with three vehicles waiting on the south approach (frontage road). All of these vehicles cleared within the green phase of the traffic signal.
- There was one time (5:15 P.M.) in which the westbound queues extended beyond the provided storage area and it was observed that six vehicles were queued on the frontage road. All of these vehicles cleared within the green phase of the traffic signal.

Traffic Characteristics of the Proposed Development

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed development, including the directional distribution and volume of traffic that it will generate.

Proposed Development Plan

As proposed, the site will be developed as a four-story, 124-unit hotel. The hotel will provide approximately 129 parking spaces with a drop-off/pick-up area at the front of the building.

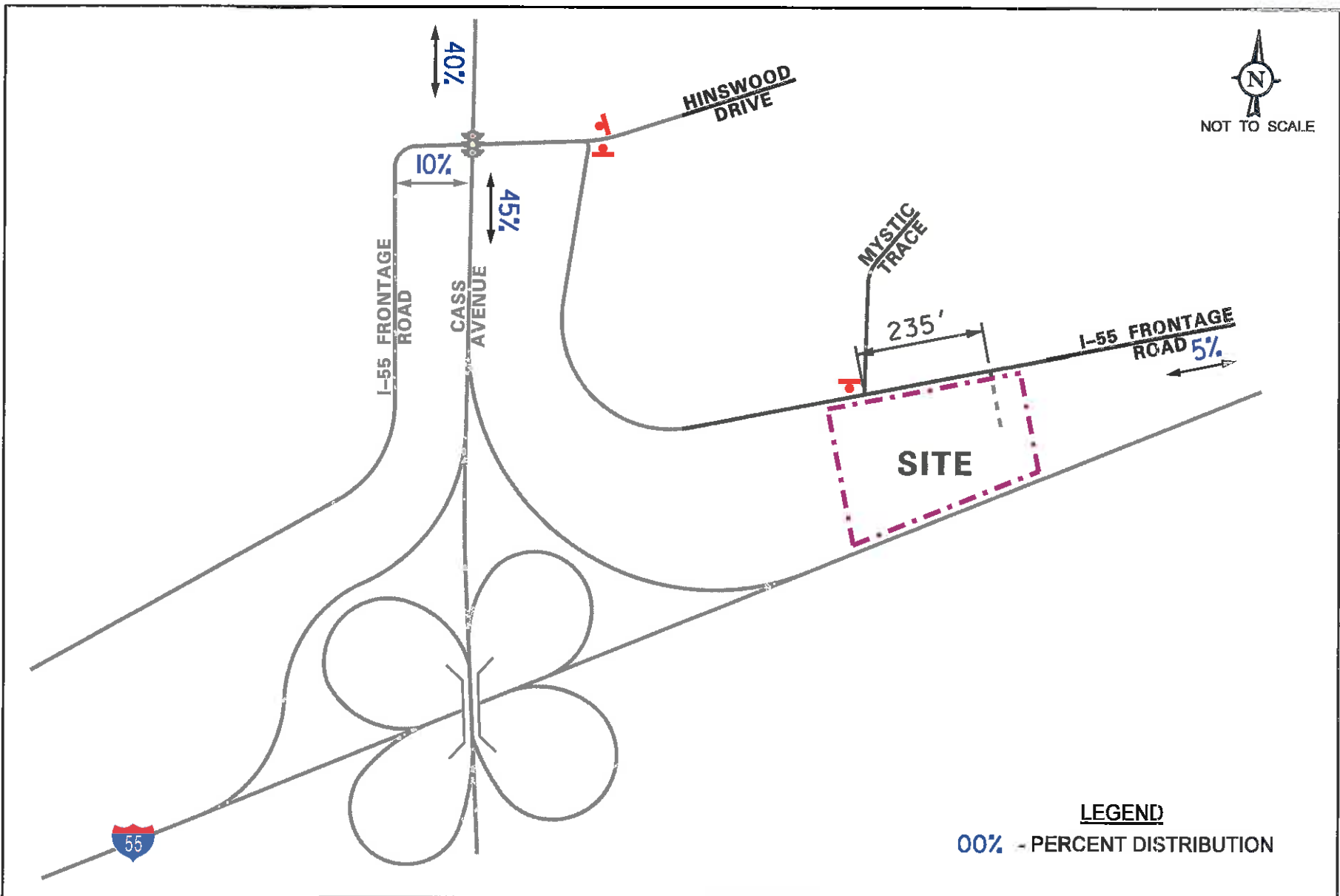
Access to the proposed development will be provided on the south side of the I-55 north frontage road via one full ingress/egress access drive located approximately 235 feet east of Mystic Trace. The access drive will provide one inbound lane and one outbound lane with outbound movements under stop sign control.

Directional Distribution

The directional distribution for the proposed development was estimated based on the location of the site relative to the primary roadway system and the existing travel patterns as determined from the traffic counts. **Figure 5** illustrates the directional distribution.

Trip Generation Estimate

The volume of traffic generated by a development is based on the type of land use and the size of the development. The number of peak hour trips that will be generated by the proposed development was estimated based on trip rates published by the Institute of Transportation Engineers (ITE) in its 9th Edition of the *Trip Generation Manual*. **Table 1** shows the peak hour traffic to be generated by the proposed hotel development during the peak hours and on a daily basis.



PROJECT:
 I-55 Frontage Road
 Hotel Development
 Darien, Illinois

TITLE:
 Estimated Directional Distribution

KLOA
 Job No: 15-046
 Figure: 5

Table 1
ESTIMATED SITE-GENERATED TRAFFIC VOLUMES

ITE Land- Use Code	Type/Size	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily Trips		
		In	Out	Total	In	Out	Total	In	Out	Total
310	Hotel (124 units)	39	27	66	38	36	74	368	368	736

Traffic Assignment

The estimated new weekday morning and evening peak hour trips that will be generated by the proposed development were assigned to the roadway system in accordance with the previously described directional distribution and illustrated in **Figure 6**. To account for other growth in the area, a background traffic growth rate was determined based on the Chicago Metropolitan Agency for Planning (CMAP) population projections. The growth rate was applied over a six year period, increasing traffic by a total of four percent. **Figure 7** illustrates the total future traffic volumes taking into account the traffic generated by the development as well as background growth.

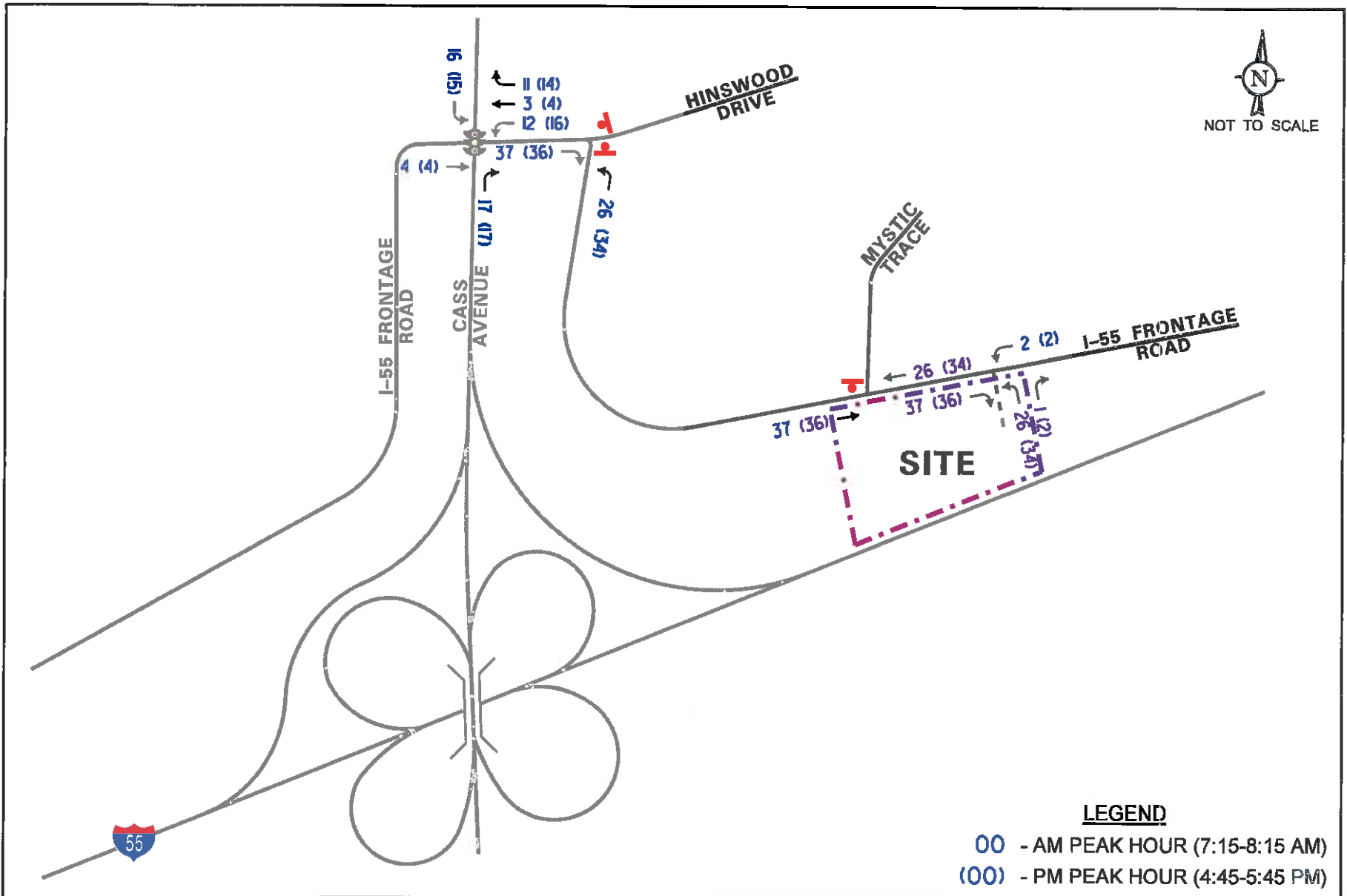
Traffic Analysis

Traffic analyses were performed for the intersections within the study area to determine the operation of the existing roadway system, evaluate the impact of the proposed development, and determine the ability of the existing roadway system to accommodate projected traffic demands. Analyses were performed for the weekday morning and evening peak hours for the existing and projected traffic volumes.

The traffic analyses were performed using the Synchro/SimTraffic 9 computer software, which is based on the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM), 2010*. The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter grade from A to F based on the average control delay experienced by vehicles passing through the intersection. Control delay is that portion of the total delay attributed to the traffic signal or stop sign control operation and includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. Level of Service A is the highest grade (best traffic flow and least delay), Level of Service E represents saturated or at-capacity conditions, and Level of Service F is the lowest grade (oversaturated conditions, extensive delays).

For two-way stop controlled (TWSC) intersections, levels of service are only calculated for the approaches controlled by a stop sign (not for the intersection as a whole).

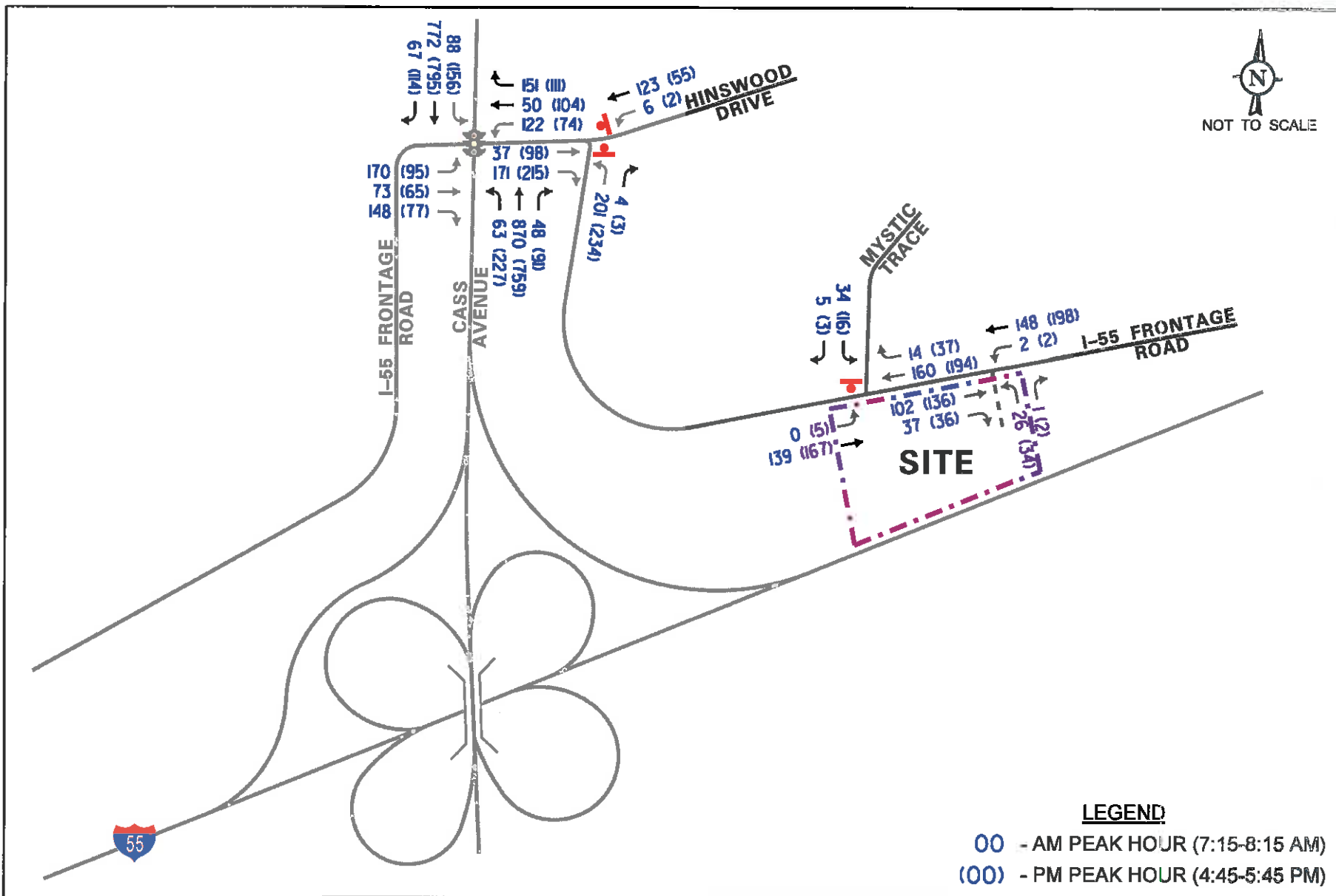
The results of the capacity analysis are summarized in **Table 2** for the existing and projected traffic volumes. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for unsignalized intersections are shown in the Appendix.



PROJECT:
**I-55 Frontage Road
 Hotel Development
 Darien, Illinois**

TITLE:
Estimated Site-Generated Traffic Volumes

KLOA
 Job No: 15-046
 Figure: 6



PROJECT:
 I-55 Frontage Road
 Hotel Development
 Darien, Illinois

TITLE:
 Total Traffic Volumes

KLOA
 Job No: 15-046
 Figure: 7

Table 2
CAPACITY ANALYSIS RESULTS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Existing Conditions				
Cass Avenue and I-55 North Frontage Road/Hinswood Drive ¹	C	23.7	C	21.0
I-55 North Frontage Road and Hinswood Drive ^{2,3}	A	--	A	--
I-55 North Frontage Road and Mystic Trace ²	B	10.1	B	10.4
Projected Conditions				
Cass Avenue and I-55 North Frontage Road/Hinswood Drive ¹	C	26.0	C	24.2
I-55 North Frontage Road and Hinswood Drive ^{2,3}	A	--	A	--
I-55 North Frontage Road and Mystic Trace ²	B	10.6	B	11.1
I-55 North Frontage Road and Proposed Access Drive ²	B	10.4	B	11.1
LOS - Level of Service				
Delay - Measured in seconds				
¹ - Signalized Intersection				
² - Stop-Sign-Controlled Intersection				
³ - Two of the three approaches at this intersection are under stop sign control with the third approach uncontrolled. Therefore, HCM delay cannot be determined. The operation of these intersections is based on a critical volume to saturation flow (v/s) evaluation also known as the Intersection Capacity Utilization (ICU) method.				

Traffic Evaluation

The following section summarizes the results of the traffic analysis for both intersections within the study area. The capacity analysis reports are included in the Appendix.

Cass Avenue and I-55 North Frontage Road/Hinswood Drive

The results of the capacity analysis indicate that the intersection currently operates at satisfactory Levels of Service C during the weekday morning and evening peak hours. Under projected conditions, the intersection will continue to operate at the same levels of service with minimal increases in the overall delay. Furthermore, inspection of the projected traffic volumes indicate that the development generated traffic will amount to an approximate two percent increase in the existing peak hour traffic volumes at the intersection of Cass Avenue and the frontage road.

In addition to the capacity analyses, KLOA, Inc. ran several simulation models of the existing and projected traffic conditions. The existing condition simulation models indicated that from time to time westbound traffic will queue beyond the storage area and traffic on the east and south approach will have to wait to enter the intersection which was observed in the field. However, as it was also observed, these queues cleared with every green phase of the traffic signal validating the simulation model. Under projected conditions and based on a review of the simulation model, maximum queues will increase by one or two vehicles and will continue to clear with every green phase of the traffic signal thus indicating that the proposed development will have minimal impact on traffic conditions at this intersection. As such, no geometric or signal timing improvements are necessary to accommodate future traffic volumes.

I-55 North Frontage Road and Hinswood Drive

The existing intersection of I-55 North Frontage Road and Hinswood Drive is a three legged intersection with the westbound and northbound approaches under stop sign control therefore allowing the eastbound approach to operate under free flow conditions in order to prevent back-ups onto Cass Avenue. It should be noted that this traffic control configuration cannot be analyzed using typical HCM procedures. Given this configuration and HCM limitations, the intersection was analyzed using the intersection capacity utilization (ICU) level of service. The ICU indicates how much reserve capacity is available or how much an intersection is overcapacity. Based on the ICU analysis, the intersection is and will continue utilizing less than 40 percent of the capacity of the intersection. Further, as discussed above, field observations and simulation models confirm that this intersection is currently operating well with limited delays and queues. Therefore, no geometric or traffic control improvements will be necessary to accommodate future traffic volumes.

I-55 North Frontage Road and Mystic Trace

The results of the capacity analysis indicate that this intersection currently operates at a Level of Service B during the morning and evening peak hours. Under projected conditions, this intersection will continue to operate at the same levels of service. As such, no geometric or traffic control improvements will be necessary to accommodate future traffic volumes.

I-55 North Frontage Road and Proposed Access Drive

As proposed, the development will be served by a full ingress/egress access drive located approximately 235 feet east of Mystic Trace. As previously indicated, the access drive will provide one inbound lane and one outbound lane with the outbound movements under stop sign control. Based on the results of the capacity analyses, the intersection will operate at a Level of Service B during the morning and evening peak hours. Based on a review of the projected traffic volumes and the requirements set forth in IDOT's Bureau of Design and Environment (BDE) Manual, an exclusive left-turn lane or an exclusive right-turn lane will not be required on the I-55 north frontage road serving the access drive.

Parking Evaluation

The proposed development will provide 124 hotel rooms, five employees and 129 off-street parking spaces. Based on a review of the City of Darien Zoning Ordinance, the hotel should provide one parking space per room and one parking space per employee for a total 129 parking spaces. Therefore, the proposed development meets the City of Darien parking requirements. Based on a review of the Institute of Transportation Engineers (ITE) Parking Generation Manual, 4th Edition, a 124 room hotel will have a weekday peak parking demand of 77 parking spaces. As such, the proposed development's parking supply meets the City's requirement and will be adequate in accommodating the anticipated peak parking demand.

A review of the Illinois Vehicle Accessibility Code indicates that a development with 129 parking spaces should provide at a minimum five (5) handicapped accessible parking spaces. Based on a review of the site plan, the proposed development is providing five (5) handicapped parking spaces therefore meeting the requirement.

Conclusions and Recommendations

Based on the preceding analyses and recommendations, the following conclusions and recommendations were made.

- The development will generate a low volume of traffic and as a result will not have a significant impact on area roadways.
- The results of the capacity analyses have shown that the existing roadway system is sufficient to accommodate the traffic to be generated by the development.
- The intersection of Cass Avenue with the frontage road is and will continue operating at acceptable levels of service.
- The westbound maximum queues currently experienced on the frontage road at its intersection with Cass Avenue clear with every green phase and will not be exacerbated by the proposed development.
- A single access drive in and out of the development will be able to sufficiently accommodate all development traffic. This access drive will provide one inbound lane and one outbound lane with outbound movements under stop sign control.

Appendix

- **Turning Movement Count Data**
- **Level of Service Table**
- **Capacity Analysis Reports**

LEVEL OF SERVICE CRITERIA

Signalized Intersections

Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤10
B	Good progression, with more vehicles stopping than for Level of Service A.	>10 - 20
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	>20 - 35
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	>35 - 55
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	>55 - 80
F	The volume-to-capacity ratio is very high, progression is very poor and the cycle length is long. Most cycles fail to clear the queue.	>80.0

Unsignalized Intersections

Level of Service	Average Total Delay (SEC/VEH)
A	0 - 10
B	> 10 - 15
C	> 15 - 25
D	> 25 - 35
E	> 35 - 50
F	> 50

Source: *Highway Capacity Manual*, 2010.

Turning Movement Count Data



Kenig Lindgren O'Hara Aboona, Inc.
9575 W. Higgins Rd., Suite 400

Rosemont, Illinois, United States 60018
(847)518-9990

Count Name: Cass Ave/Hinswood
Site Code:
Start Date: 02/24/2015
Page No: 1

Turning Movement Data

Start Time	I-55 N Frontage Road Eastbound						Hinswood Drive Westbound						Cass Avenue Northbound						Cass Avenue Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:00 AM	0	24	12	35	0	71	0	12	3	27	0	42	0	11	185	7	0	183	0	12	136	5	0	153	449
7:15 AM	0	39	22	38	0	99	0	38	12	28	0	78	0	21	206	5	0	232	0	14	182	12	0	208	617
7:30 AM	0	41	22	27	0	90	0	31	11	36	0	78	0	17	227	9	0	253	0	16	211	12	0	239	680
7:45 AM	0	45	11	39	0	95	0	18	14	34	0	66	0	12	228	9	0	249	0	16	176	10	0	202	612
Hourly Total	0	149	67	139	0	355	0	99	40	125	0	264	0	61	826	30	0	917	0	58	705	39	0	802	2338
8:00 AM	0	38	4	38	0	80	0	19	8	37	0	64	0	11	176	7	0	194	0	23	173	30	0	226	564
8:15 AM	0	27	13	29	0	69	0	22	16	26	0	64	1	27	146	8	0	182	0	13	174	16	0	203	518
8:30 AM	0	29	20	38	0	87	0	25	8	21	0	54	0	15	140	8	0	163	0	16	177	27	0	220	524
8:45 AM	0	17	2	31	0	50	0	20	11	18	0	49	0	17	141	10	0	168	0	15	137	31	0	183	450
Hourly Total	0	111	39	136	0	286	0	86	43	102	0	231	1	70	603	33	0	707	0	67	661	104	0	832	2056
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	0	18	8	14	0	40	0	14	18	28	0	60	1	31	114	9	0	155	0	23	180	22	0	225	480
4:15 PM	0	24	15	18	0	57	0	16	17	32	0	65	0	36	122	10	0	168	0	28	194	24	0	246	538
4:30 PM	0	25	15	26	0	66	0	14	17	22	0	53	0	37	143	10	0	190	0	15	205	33	0	253	562
4:45 PM	0	23	18	21	0	62	0	9	17	23	0	49	1	37	171	16	0	225	0	34	211	21	0	266	602
Hourly Total	0	90	58	79	0	225	0	53	69	105	0	227	2	141	550	45	0	738	0	100	790	100	0	990	2180
5:00 PM	0	17	20	19	0	56	0	10	21	22	0	53	0	52	179	23	0	254	0	27	196	41	0	264	627
5:15 PM	0	33	12	22	0	67	0	24	26	26	1	76	0	61	192	18	0	271	0	32	192	24	2	248	652
5:30 PM	0	18	9	12	0	39	0	13	32	22	0	67	1	66	188	14	0	269	0	43	185	24	0	232	607
5:45 PM	0	15	16	15	0	46	0	9	22	21	0	52	1	36	187	23	0	247	0	31	193	27	0	251	586
Hourly Total	0	83	57	68	0	208	0	56	101	91	1	248	2	215	746	78	0	1041	0	133	746	113	2	995	2492
Grand Total	0	433	219	422	0	1074	0	294	253	423	1	970	5	487	2725	186	0	3403	0	358	2902	359	2	3619	9066
Approach %	0.0	40.3	20.4	39.3	-	-	0.0	30.3	26.1	43.6	-	-	0.1	14.3	80.1	5.5	-	-	0.0	9.9	80.2	9.9	-	-	-
Total %	0.0	4.8	2.4	4.7	-	11.8	0.0	3.2	2.8	4.7	-	10.7	0.1	5.4	30.1	2.1	-	37.5	0.0	3.9	32.0	4.0	-	39.9	-
Lights	0	417	210	419	-	1046	0	292	243	408	-	943	5	475	2646	178	-	3304	0	347	2628	350	-	3626	8818
% Lights	-	96.3	95.9	99.3	-	97.4	-	88.3	96.0	96.5	-	97.2	100.0	97.5	97.1	95.7	-	97.1	-	96.9	97.5	97.5	-	97.4	97.3
Buses	0	12	6	0	-	18	0	1	6	6	-	13	0	5	18	1	-	24	0	5	31	8	-	44	98
% Buses	-	2.8	2.7	0.0	-	1.7	-	0.3	2.4	1.4	-	1.3	0.0	1.0	0.7	0.5	-	0.7	-	1.4	1.1	2.2	-	1.2	1.1
Single-Unit Trucks	0	4	2	2	-	8	0	1	4	7	-	12	0	7	48	7	-	62	0	6	34	1	-	41	123
% Single-Unit Trucks	-	0.9	0.9	0.5	-	0.7	-	0.3	1.6	1.7	-	1.2	0.0	1.4	1.8	3.8	-	1.8	-	1.7	1.2	0.3	-	1.1	1.4
Articulated Trucks	0	0	1	1	-	2	0	0	0	2	-	2	0	0	13	0	-	13	0	0	9	0	-	9	26
% Articulated Trucks	-	0.0	0.5	0.2	-	0.2	-	0.0	0.0	0.5	-	0.2	0.0	0.0	0.5	0.0	-	0.4	-	0.0	0.3	0.0	-	0.2	0.3
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	2	-	-

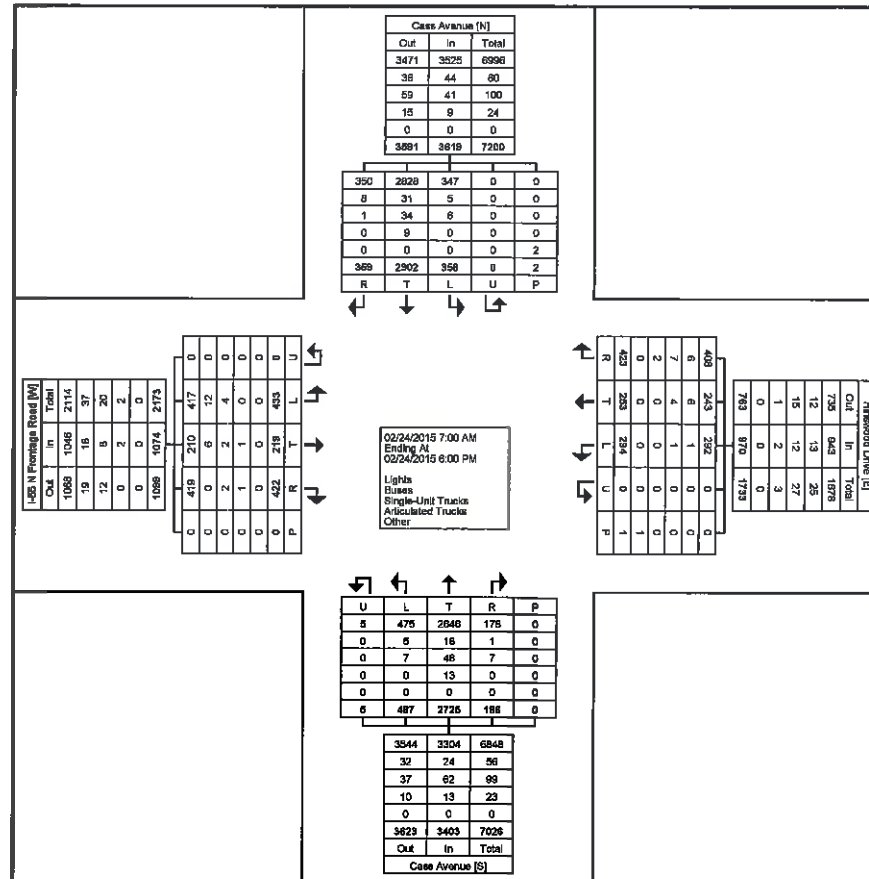


Kenig, Lindgren, O'Hara, Aboona, Inc.

Kenig Lindgren O'Hara Aboona, Inc.
9575 W. Higgins Rd., Suite 400

Rosemont, Illinois, United States 60018
(847)518-9990

Count Name: Cass Ave/Hinswood
Site Code:
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Turning Movement Data Plot



Kenig Lindgren O'Hara Aboona, Inc.
9575 W. Higgins Rd., Suite 400

Rosemont, Illinois, United States 60018
(847)518-9990

Count Name: Cass Ave/Hinswood
Site Code:
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Turning Movement Peak Hour Data (7:15 AM)

Start Time	I-55 N Frontage Road Eastbound						Hinswood Drive Westbound						Cass Avenue Northbound						Cass Avenue Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
7:15 AM	0	39	22	38	0	99	0	38	12	28	0	78	0	21	206	5	0	232	0	14	182	12	0	208	817
7:30 AM	0	41	22	27	0	90	0	31	11	38	0	78	0	17	227	9	0	253	0	16	211	12	0	239	860
7:45 AM	0	45	11	39	0	95	0	18	14	34	0	66	0	12	228	9	0	249	0	16	176	10	0	202	812
8:00 AM	0	38	4	38	0	80	0	19	8	37	0	64	0	11	176	7	0	194	0	23	173	30	0	226	564
Total	0	163	59	142	0	364	0	106	45	135	0	286	0	61	837	30	0	928	0	69	742	64	0	875	2453
Approach %	0.0	44.8	16.2	39.0	-	-	0.0	37.1	15.7	47.2	-	-	0.0	6.8	90.2	3.2	-	-	0.0	7.9	84.8	7.3	-	-	-
Total %	0.0	8.6	2.4	5.8	-	14.8	0.0	4.3	1.8	5.5	-	11.7	0.0	2.5	34.1	1.2	-	37.8	0.0	2.8	30.2	2.6	-	35.7	-
PHF	0.000	0.906	0.670	0.910	-	0.919	0.000	0.697	0.804	0.912	-	0.917	0.000	0.726	0.918	0.833	-	0.917	0.000	0.750	0.879	0.533	-	0.915	0.929
Lights	0	158	56	142	-	356	0	106	43	127	-	276	0	61	808	28	-	897	0	62	712	60	-	834	2363
% Lights	-	96.9	94.9	100.0	-	97.8	-	100.0	95.6	94.1	-	96.5	-	100.0	96.5	93.3	-	96.7	-	89.9	96.0	93.8	-	95.3	96.3
Buses	0	4	3	0	-	7	0	0	2	4	-	6	0	0	6	1	-	7	0	4	17	4	-	25	45
% Buses	-	2.5	5.1	0.0	-	1.9	-	0.0	4.4	3.0	-	2.1	-	0.0	0.7	3.3	-	0.8	-	5.8	2.3	6.3	-	2.9	1.8
Single-Unit Trucks	0	1	0	0	-	1	0	0	0	3	-	3	0	0	17	1	-	18	0	3	9	0	-	12	34
% Single-Unit Trucks	-	0.6	0.0	0.0	-	0.3	-	0.0	0.0	2.2	-	1.0	-	0.0	2.0	3.3	-	1.9	-	4.3	1.2	0.0	-	1.4	1.4
Articulated Trucks	0	0	0	0	-	0	0	0	0	1	-	1	0	0	6	0	-	6	0	0	4	0	-	4	11
% Articulated Trucks	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.7	-	0.3	-	0.0	0.7	0.0	-	0.6	-	0.0	0.5	0.0	-	0.5	0.4
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-



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Count Name: Cass Ave/Hinswood
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Start Date: 02/24/2015
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Turning Movement Peak Hour Data (4:45 PM)

Start Time	I-55 N Frontage Road Eastbound						Hinswood Drive Westbound						Cass Avenue Northbound						Cass Avenue Southbound						Int. Total
	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Right	Peds	App. Total	
4:45 PM	0	23	18	21	0	62	0	9	17	23	0	49	1	37	171	16	0	225	0	34	211	21	0	266	602
5:00 PM	0	17	20	19	0	56	0	10	21	22	0	53	0	52	179	23	0	254	0	27	196	41	0	264	627
5:15 PM	0	33	12	22	0	67	0	24	26	26	1	78	0	81	192	18	0	271	0	32	192	24	2	248	662
5:30 PM	0	18	9	12	0	39	0	13	32	22	0	67	1	66	188	14	0	269	0	43	165	24	0	232	607
Total	0	91	59	74	0	224	0	56	96	93	1	245	2	218	730	71	0	1019	0	136	764	110	2	1010	2498
Approach %	0.0	40.6	26.3	33.0	-	-	0.0	22.9	39.2	38.0	-	-	0.2	21.2	71.6	7.0	-	-	0.0	13.5	75.6	10.9	-	-	-
Total %	0.0	3.6	2.4	3.0	-	9.0	0.0	2.2	3.8	3.7	-	9.8	0.1	8.6	29.2	2.8	-	40.8	0.0	5.4	30.8	4.4	-	40.4	-
PHF	0.000	0.689	0.738	0.841	-	0.836	0.000	0.583	0.750	0.894	-	0.806	0.500	0.818	0.951	0.772	-	0.940	0.000	0.791	0.905	0.671	-	0.949	0.943
Lights	0	90	58	74	-	222	0	55	95	91	-	241	2	215	716	69	-	1002	0	136	756	103	-	1000	2485
% Lights	-	98.9	98.3	100.0	-	99.1	-	98.2	99.0	97.8	-	98.4	100.0	99.5	98.1	97.2	-	96.3	-	100.0	99.0	98.2	-	99.0	98.7
Buses	0	0	1	0	-	1	0	1	0	1	-	2	0	0	6	0	-	6	0	0	4	2	-	6	15
% Buses	-	0.0	1.7	0.0	-	0.4	-	1.8	0.0	1.1	-	0.8	0.0	0.0	0.8	0.0	-	0.6	-	0.0	0.5	1.8	-	0.6	0.6
Single-Unit Trucks	0	1	0	0	-	1	0	0	1	1	-	2	0	1	7	2	-	10	0	0	3	0	-	3	16
% Single-Unit Trucks	-	1.1	0.0	0.0	-	0.4	-	0.0	1.0	1.1	-	0.8	0.0	0.5	1.0	2.8	-	1.0	-	0.0	0.4	0.0	-	0.3	0.6
Articulated Trucks	0	0	0	0	-	0	0	0	0	0	-	0	0	0	1	0	-	1	0	0	1	0	-	1	2
% Articulated Trucks	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.1	0.0	-	0.1	-	0.0	0.1	0.0	-	0.1	0.1
Bicycles on Road	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	-	0.0	-	0.0	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



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Turning Movement Data

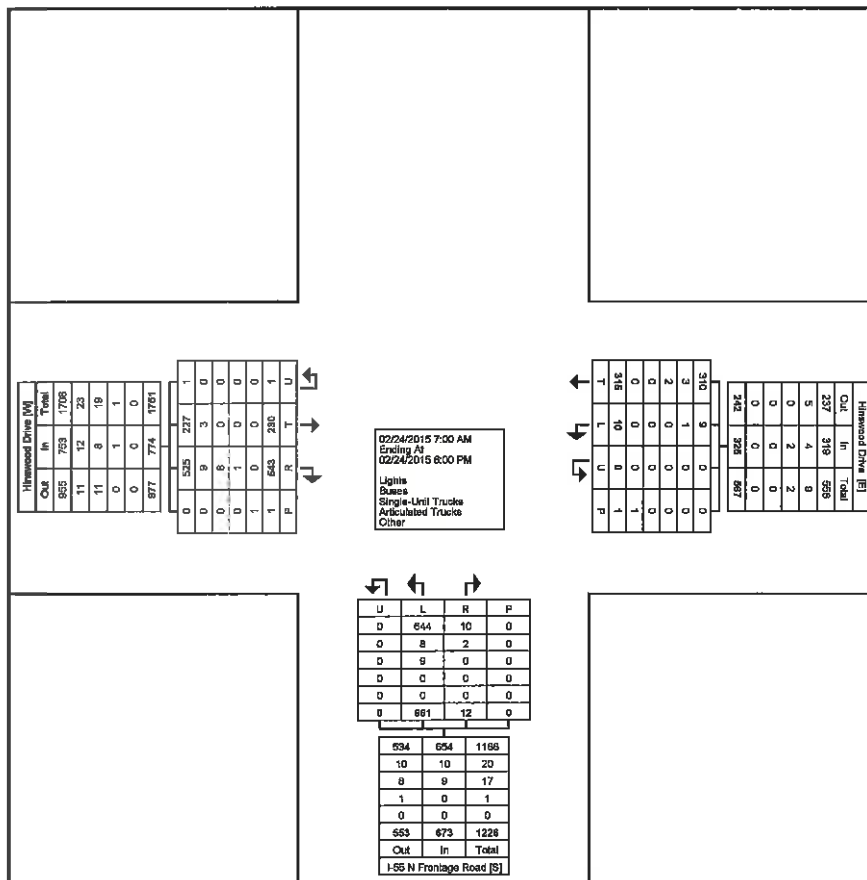
Start Time	Hinswood Drive Eastbound					Hinswood Drive Westbound					I-55 N Frontage Road Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:00 AM	0	3	27	0	30	0	0	16	0	16	0	30	1	0	31	77
7:15 AM	0	2	39	0	41	0	0	25	0	25	0	55	0	0	55	121
7:30 AM	0	7	41	0	48	0	2	31	0	33	0	47	1	0	48	129
7:45 AM	0	9	32	0	41	0	3	29	0	32	0	38	2	0	40	113
Hourly Total	0	21	139	0	160	0	5	101	0	106	0	170	4	0	174	440
8:00 AM	0	18	17	0	35	0	1	33	0	34	0	29	1	0	30	99
8:15 AM	1	9	25	0	35	0	0	31	0	31	0	37	0	0	37	103
8:30 AM	0	15	27	0	42	0	0	24	0	24	0	30	0	0	30	96
8:45 AM	0	3	25	0	28	0	0	24	0	24	0	24	0	0	24	76
Hourly Total	1	45	94	0	140	0	1	112	0	113	0	120	1	0	121	374
9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Hourly Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:00 PM	0	11	28	0	39	0	1	8	0	9	0	47	2	0	49	97
4:15 PM	0	21	35	0	56	0	0	15	0	15	0	46	1	0	47	118
4:30 PM	0	12	30	0	42	0	0	15	0	15	0	43	0	0	43	100
4:45 PM	0	20	47	0	67	0	0	13	0	13	0	37	1	0	38	116
Hourly Total	0	64	140	0	204	0	1	51	0	52	0	173	4	0	177	433
5:00 PM	0	26	46	0	72	0	0	11	0	11	0	50	1	0	51	134
5:15 PM	0	24	38	1	62	0	2	18	1	20	0	52	1	0	53	135
5:30 PM	0	24	41	0	65	0	0	11	0	11	0	57	0	0	57	133
5:45 PM	0	26	45	0	71	0	1	11	0	12	0	39	1	0	40	123
Hourly Total	0	100	170	1	270	0	3	51	1	54	0	196	3	0	201	525
Grand Total	1	230	543	1	774	0	10	315	1	325	0	661	12	0	673	1772
Approach %	0.1	29.7	70.2	-	-	0.0	3.1	96.9	-	-	0.0	98.2	1.8	-	-	-
Total %	0.1	13.0	30.6	-	43.7	0.0	0.6	17.8	-	18.3	0.0	37.3	0.7	-	38.0	-
Lights	1	227	525	-	753	0	9	310	-	319	0	644	10	-	654	1726
% Lights	100.0	98.7	96.7	-	97.3	-	90.0	98.4	-	98.2	-	97.4	83.3	-	97.2	97.4
Buses	0	3	9	-	12	0	1	3	-	4	0	8	2	-	10	26
% Buses	0.0	1.3	1.7	-	1.6	-	10.0	1.0	-	1.2	-	1.2	16.7	-	1.5	1.5
Single-Unit Trucks	0	0	8	-	8	0	0	2	-	2	0	9	0	-	9	19
% Single-Unit Trucks	0.0	0.0	1.5	-	1.0	-	0.0	0.6	-	0.6	-	1.4	0.0	-	1.3	1.1
Articulated Trucks	0	0	1	-	1	0	0	0	-	0	0	0	0	-	0	1
% Articulated Trucks	0.0	0.0	0.2	-	0.1	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.1
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	0.0	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	1	-	-	-	-	1	-	-	-	-	0	-	-



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Turning Movement Data Plot



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Count Name: I-55 Frontage/Hinswood
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Turning Movement Peak Hour Data (7:15 AM)

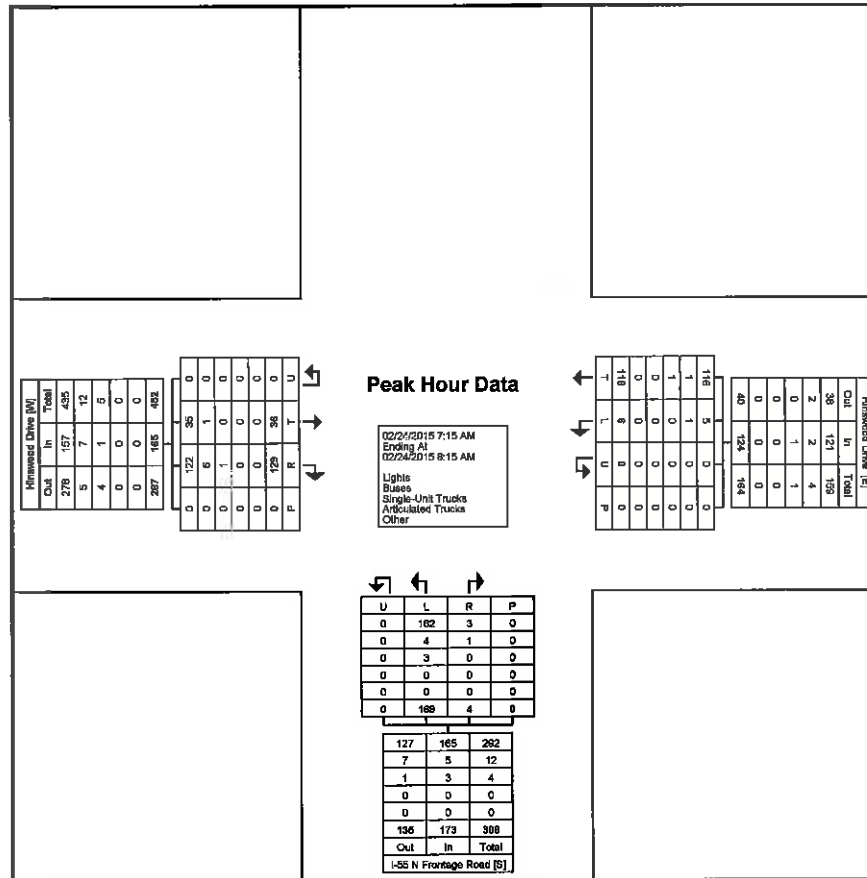
Start Time	Hinswood Drive Eastbound					Hinswood Drive Westbound					I-55 N Frontage Road Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:15 AM	0	2	39	0	41	0	0	25	0	25	0	55	0	0	55	121
7:30 AM	0	7	41	0	48	0	2	31	0	33	0	47	1	0	48	129
7:45 AM	0	9	32	0	41	0	3	29	0	32	0	38	2	0	40	113
8:00 AM	0	18	17	0	35	0	1	33	0	34	0	29	1	0	30	99
Total	0	36	129	0	165	0	6	118	0	124	0	169	4	0	173	462
Approach %	0.0	21.8	78.2	-	-	0.0	4.8	95.2	-	-	0.0	97.7	2.3	-	-	-
Total %	0.0	7.8	27.9	-	35.7	0.0	1.3	25.5	-	26.8	0.0	36.6	0.9	-	37.4	-
PHF	0.000	0.500	0.787	-	0.859	0.000	0.500	0.894	-	0.912	0.000	0.788	0.500	-	0.786	0.895
Lights	0	35	122	-	157	0	5	116	-	121	0	162	3	-	165	443
% Lights	-	97.2	94.6	-	95.2	-	83.3	98.3	-	97.6	-	95.9	75.0	-	95.4	95.9
Buses	0	1	6	-	7	0	1	1	-	2	0	4	1	-	5	14
% Buses	-	2.8	4.7	-	4.2	-	16.7	0.8	-	1.6	-	2.4	25.0	-	2.9	3.0
Single-Unit Trucks	0	0	1	-	1	0	0	1	-	1	0	3	0	-	3	5
% Single-Unit Trucks	-	0.0	0.8	-	0.6	-	0.0	0.8	-	0.8	-	1.8	0.0	-	1.7	1.1
Articulated Trucks	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Articulated Trucks	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-



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Turning Movement Peak Hour Data Plot (7:15 AM)



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Turning Movement Peak Hour Data (4:45 PM)

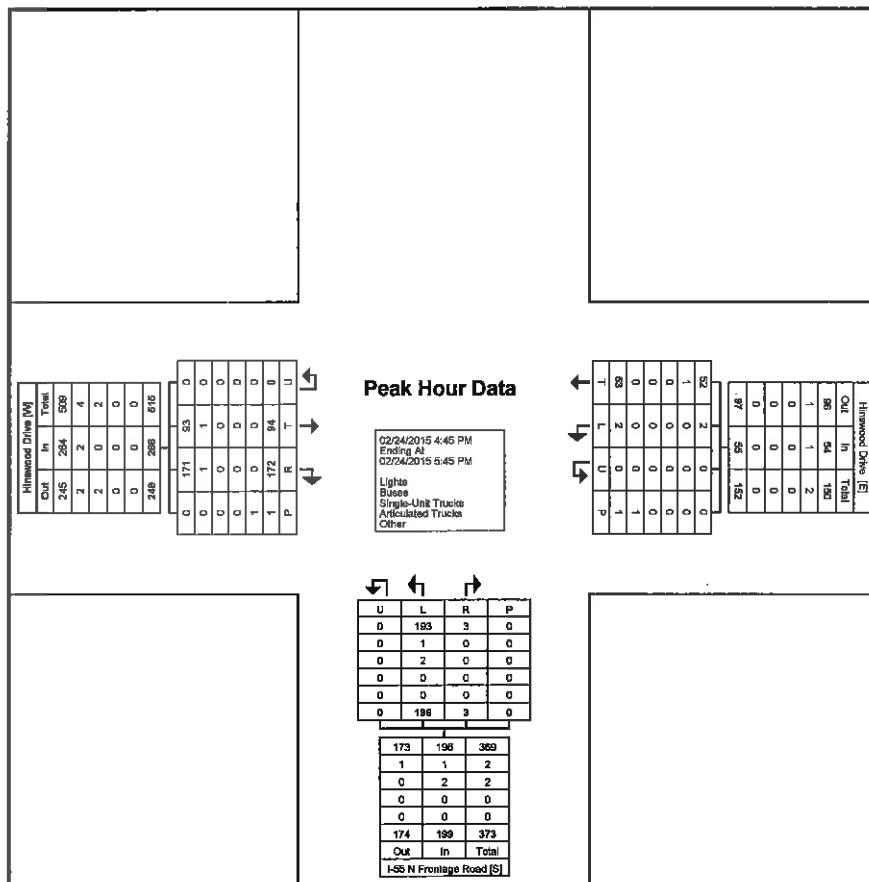
Start Time	Hinswood Drive Eastbound					Hinswood Drive Westbound					I-55 N Frontage Road Northbound					Int. Total
	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
4:45 PM	0	20	47	0	67	0	0	13	0	13	0	37	1	0	38	118
5:00 PM	0	26	46	0	72	0	0	11	0	11	0	50	1	0	51	134
5:15 PM	0	24	38	1	62	0	2	18	1	20	0	52	1	0	53	135
5:30 PM	0	24	41	0	65	0	0	11	0	11	0	57	0	0	57	133
Total	0	94	172	1	266	0	2	53	1	55	0	196	3	0	199	520
Approach %	0.0	35.3	64.7	-	-	0.0	3.6	96.4	-	-	0.0	98.5	1.5	-	-	-
Total %	0.0	18.1	33.1	-	51.2	0.0	0.4	10.2	-	10.6	0.0	37.7	0.6	-	38.3	-
PHF	0.000	0.904	0.915	-	0.924	0.000	0.250	0.736	-	0.688	0.000	0.960	0.750	-	0.873	0.963
Lights	0	93	171	-	264	0	2	52	-	54	0	193	3	-	196	514
% Lights	-	98.9	99.4	-	99.2	-	100.0	98.1	-	98.2	-	98.5	100.0	-	98.5	98.8
Buses	0	1	1	-	2	0	0	1	-	1	0	1	0	-	1	4
% Buses	-	1.1	0.6	-	0.8	-	0.0	1.9	-	1.8	-	0.5	0.0	-	0.5	0.6
Single-Unit Trucks	0	0	0	-	0	0	0	0	-	0	0	2	0	-	2	2
% Single-Unit Trucks	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	1.0	0.0	-	1.0	0.4
Articulated Trucks	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Articulated Trucks	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	1	-	-	-	-	1	-	-	-	-	0	-	-
% Pedestrians	-	-	-	100.0	-	-	-	-	100.0	-	-	-	-	-	-	-



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 Site Code:
 Start Date: 02/24/2015
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Turning Movement Peak Hour Data Plot (4:45 PM)



Kenig Lindgren O'Hara Aboona, Inc.
 9575 W. Higgins Rd., Suite 400
 Rosemont, Illinois, United States 60018
 (847)518-9990

Count Name: I-55 Frontage/Mystic Trace
 Site Code:
 Start Date: 02/24/2015
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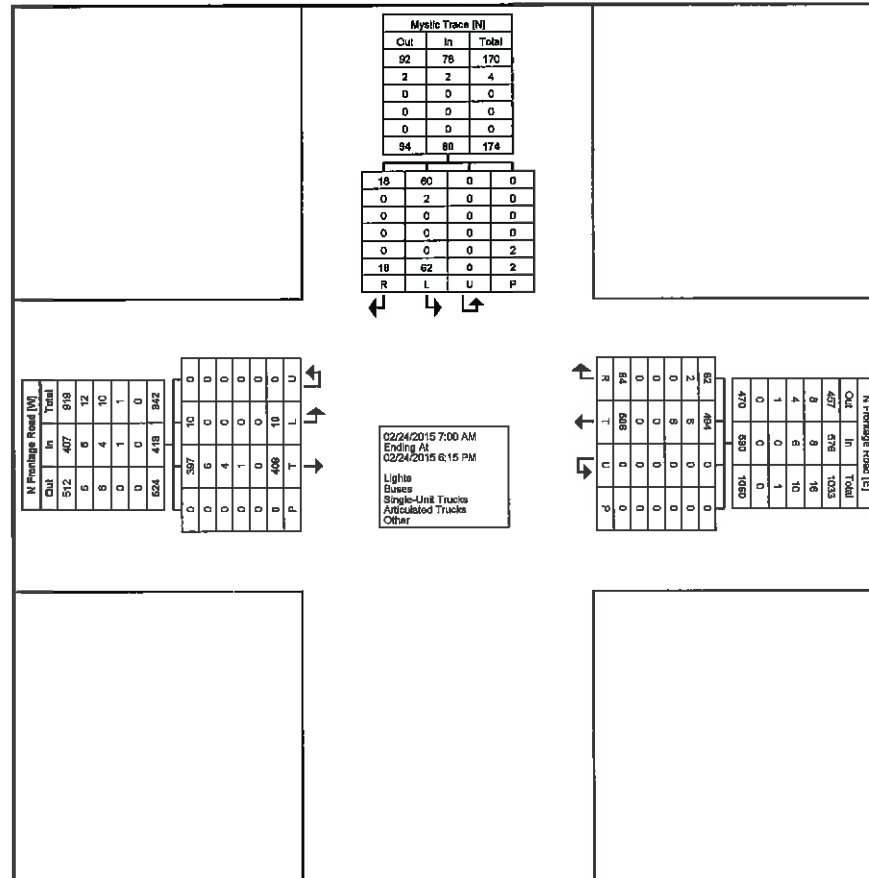
Turning Movement Data

Start Time	N Frontage Road Eastbound					N Frontage Road Westbound					Mystic Trace Southbound					Int. Total
	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:00 AM	0	0	17	0	17	0	18	0	0	18	0	2	3	0	5	40
7:15 AM	0	0	25	0	25	0	46	0	0	46	0	10	0	0	10	81
7:30 AM	0	0	26	0	26	0	31	6	0	37	0	7	2	0	9	74
7:45 AM	0	0	30	0	30	0	30	4	0	34	0	12	3	0	15	79
Hourly Total	0	0	100	0	100	0	125	10	0	135	0	31	8	0	39	274
8:00 AM	0	0	15	0	15	0	22	3	0	25	0	4	0	0	4	44
8:15 AM	0	1	15	0	16	0	24	2	0	26	0	0	0	0	0	42
8:30 AM	0	0	22	0	22	0	24	1	0	25	0	2	2	1	4	51
8:45 AM	0	1	18	0	19	0	19	0	0	19	0	2	2	0	4	42
Hourly Total	0	2	70	0	72	0	89	6	0	95	0	8	4	1	12	179
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
4:00 PM	0	1	23	0	24	0	36	8	0	44	0	4	1	0	5	73
4:15 PM	0	0	30	0	30	0	37	10	0	47	0	0	2	0	2	79
4:30 PM	0	2	26	0	28	0	27	8	0	35	0	2	0	0	2	65
4:45 PM	0	1	34	0	35	0	28	10	0	38	0	5	1	0	6	79
Hourly Total	0	4	113	0	117	0	128	36	0	164	0	11	4	0	15	296
5:00 PM	0	3	32	0	35	0	44	8	0	52	0	5	1	0	6	93
5:15 PM	0	0	32	0	32	0	39	9	0	48	0	4	1	1	5	85
5:30 PM	0	1	26	0	29	0	43	9	0	52	0	1	0	0	1	62
5:45 PM	0	0	33	0	33	0	38	6	0	44	0	2	0	0	2	79
Hourly Total	0	4	125	0	129	0	164	32	0	196	0	12	2	1	14	339
6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	10	408	0	418	0	506	84	0	590	0	62	18	2	80	1086
Approach %	0.0	2.4	97.6	-	-	0.0	85.8	14.2	-	-	0.0	77.5	22.5	-	-	-
Total %	0.0	0.9	37.5	-	38.4	0.0	46.5	7.7	-	54.2	0.0	5.7	1.7	-	7.4	-
Lights	0	10	397	-	407	0	494	62	-	576	0	60	18	-	78	1061
% Lights	-	100.0	97.3	-	97.4	-	97.6	97.6	-	97.6	-	96.8	100.0	-	97.5	97.5
Buses	0	0	6	-	6	0	6	2	-	8	0	2	0	-	2	16
% Buses	-	0.0	1.5	-	1.4	-	1.2	2.4	-	1.4	-	3.2	0.0	-	2.5	1.5
Single-Unit Trucks	0	0	4	-	4	0	6	0	-	6	0	0	0	-	0	10
% Single-Unit Trucks	-	0.0	1.0	-	1.0	-	1.2	0.0	-	1.0	-	0.0	0.0	-	0.0	0.9
Articulated Trucks	0	0	1	-	1	0	0	0	-	0	0	0	0	-	0	1
% Articulated Trucks	-	0.0	0.2	-	0.2	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.1
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	2	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-	-



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Turning Movement Data Plot



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Count Name: I-55 Frontage/Mystic Trace
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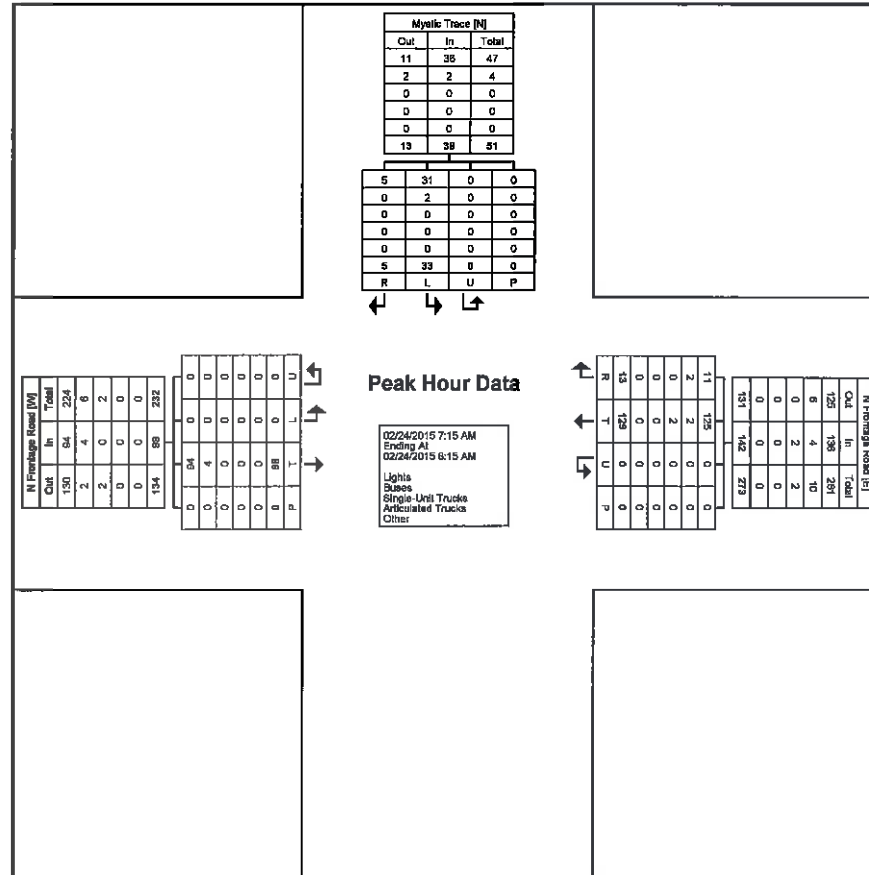
Turning Movement Peak Hour Data (7:15 AM)

Start Time	N Frontage Road Eastbound					N Frontage Road Westbound					Mystic Trace Southbound					Int. Total
	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
7:15 AM	0	0	25	0	25	0	46	0	0	46	0	10	0	0	10	81
7:30 AM	0	0	28	0	28	0	31	6	0	37	0	7	2	0	9	74
7:45 AM	0	0	30	0	30	0	30	4	0	34	0	12	3	0	15	79
8:00 AM	0	0	15	0	15	0	22	3	0	25	0	4	0	0	4	44
Total	0	0	98	0	98	0	129	13	0	142	0	33	5	0	38	278
Approach %	0.0	0.0	100.0	-	-	0.0	90.8	9.2	-	-	0.0	86.8	13.2	-	-	-
Total %	0.0	0.0	35.3	-	35.3	0.0	46.4	4.7	-	51.1	0.0	11.9	1.8	-	13.7	-
PHF	0.000	0.000	0.817	-	0.817	0.000	0.701	0.542	-	0.772	0.000	0.688	0.417	-	0.633	0.858
Lights	0	0	94	-	94	0	125	11	-	136	0	31	5	-	36	286
% Lights	-	-	95.9	-	95.9	-	96.9	84.6	-	95.8	-	83.9	100.0	-	94.7	95.7
Buses	0	0	4	-	4	0	2	2	-	4	0	2	0	-	2	10
% Buses	-	-	4.1	-	4.1	-	1.6	15.4	-	2.8	-	6.1	0.0	-	5.3	3.6
Single-Unit Trucks	0	0	0	-	0	0	2	0	-	2	0	0	0	-	0	2
% Single-Unit Trucks	-	-	0.0	-	0.0	-	1.6	0.0	-	1.4	-	0.0	0.0	-	0.0	0.7
Articulated Trucks	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Articulated Trucks	-	-	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	-	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-



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Turning Movement Peak Hour Data Plot (7:15 AM)



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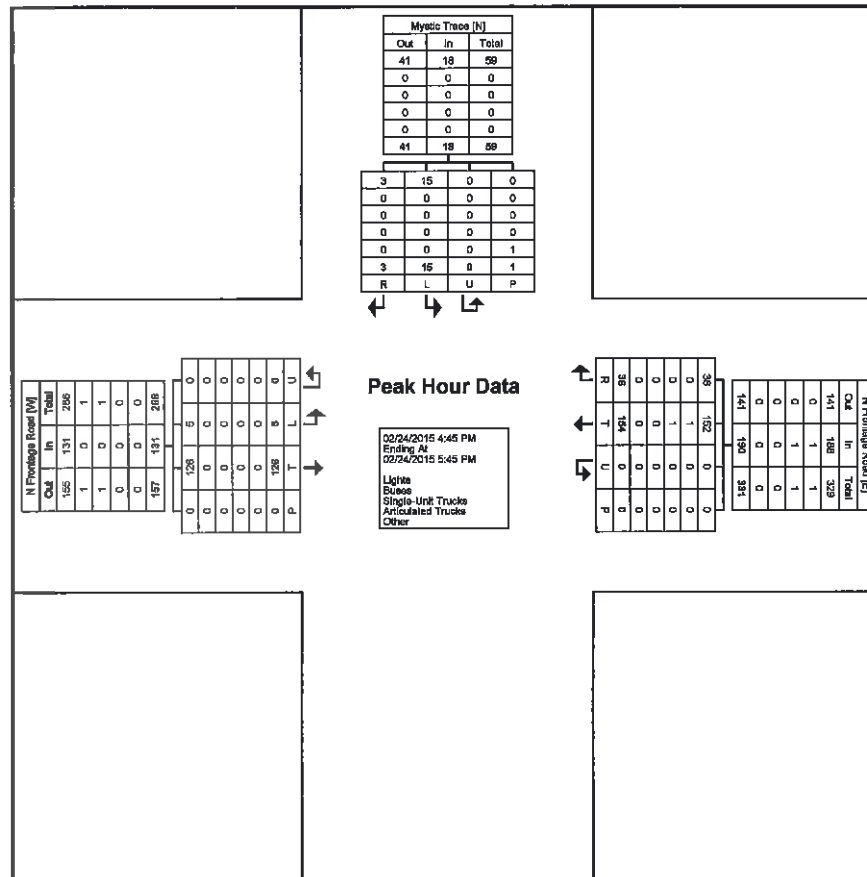
Turning Movement Peak Hour Data (4:45 PM)

Start Time	N Frontage Road Eastbound					N Frontage Road Westbound					Mystic Trace Southbound					Int. Total
	U-Turn	Left	Thru	Peds	App. Total	U-Turn	Thru	Right	Peds	App. Total	U-Turn	Left	Right	Peds	App. Total	
4:45 PM	0	1	34	0	35	0	28	10	0	38	0	5	1	0	6	79
5:00 PM	0	3	32	0	35	0	44	8	0	52	0	5	1	0	6	93
5:15 PM	0	0	32	0	32	0	39	9	0	48	0	4	1	1	5	85
5:30 PM	0	1	28	0	29	0	43	9	0	52	0	1	0	0	1	82
Total	0	5	126	0	131	0	154	36	0	190	0	15	3	1	18	339
Approach %	0.0	3.8	96.2	-	-	0.0	81.1	18.9	-	-	0.0	83.3	16.7	-	-	-
Total %	0.0	1.5	37.2	-	38.6	0.0	45.4	10.6	-	56.0	0.0	4.4	0.9	-	5.3	-
PHF	0.000	0.417	0.926	-	0.936	0.000	0.875	0.900	-	0.913	0.000	0.750	0.750	-	0.750	0.911
Lights	0	5	126	-	131	0	152	36	-	188	0	15	3	-	18	337
% Lights	-	100.0	100.0	-	100.0	-	98.7	100.0	-	98.9	-	100.0	100.0	-	100.0	99.4
Buses	0	0	0	-	0	0	1	0	-	1	0	0	0	-	0	1
% Buses	-	0.0	0.0	-	0.0	-	0.6	0.0	-	0.5	-	0.0	0.0	-	0.0	0.3
Single-Unit Trucks	0	0	0	-	0	0	1	0	-	1	0	0	0	-	0	1
% Single-Unit Trucks	-	0.0	0.0	-	0.0	-	0.6	0.0	-	0.5	-	0.0	0.0	-	0.0	0.3
Articulated Trucks	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Articulated Trucks	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Bicycles on Road	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0	0
% Bicycles on Road	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	-	0.0	0.0	-	0.0	0.0
Pedestrians	-	-	-	0	-	-	-	-	0	-	-	-	-	1	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	100.0	-	-



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Turning Movement Peak Hour Data Plot (4:45 PM)

Capacity Analysis Reports

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	163	66	142	106	45	135	61	837	30	69	742	64
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		100	0		0	185		0	155		0
Storage Lanes	0		1	0		1	1		0	1		0
Taper Length (ft)	25			25			160			170		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt			0.850				0.850		0.995		0.988	
Fit Protected		0.966			0.966		0.950			0.950		
Satd Flow (prot)	0	1799	1583	0	1799	1583	1770	3522	0	1770	3497	0
Fit Permitted		0.598			0.442		0.271			0.244		
Satd Flow (perm)	0	1114	1583	0	823	1583	505	3522	0	455	3497	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd Flow (RTOR)			135			145		4			11	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		249			172			408			402	
Travel Time (s)		5.7			3.9			9.3			9.1	
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	246	153	0	162	145	66	932	0	74	867	0
Turn Type	Perm	NA	Perm	Perm	NA	Perm	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4		4	8		8	2			6		
Detector Phase	4	4	4	8	8	8	5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	
Minimum Split (s)	21.0	21.0	21.0	21.0	21.0	21.0	9.0	21.0		9.0	21.0	
Total Split (s)	42.0	42.0	42.0	42.0	42.0	42.0	13.0	65.0		13.0	65.0	
Total Split (%)	35.0%	35.0%	35.0%	35.0%	35.0%	35.0%	10.8%	54.2%		10.8%	54.2%	
Yellow Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)		5.0	5.0		5.0	5.0	5.0	5.0		5.0	5.0	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None	None	None	None	None	None	C-Max		None	C-Max	
Act Effct Green (s)		29.6	29.6		29.6	29.6	76.2	70.4		76.5	70.5	
Actuated g/C Ratio		0.25	0.25		0.25	0.25	0.64	0.59		0.64	0.59	
v/c Ratio		0.89	0.31		0.80	0.29	0.17	0.45		0.20	0.42	
Control Delay		75.9	8.9		68.8	6.5	9.4	16.6		9.7	15.9	
Queue Delay		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		75.9	8.9		68.8	6.5	9.4	16.6		9.7	15.9	
LOS		E	A		E	A	A	B		A	B	
Approach Delay		50.2			39.4			16.1			15.4	
Approach LOS		D			D			B			B	
Queue Length 50th (ft)		183	11		117	0	16	215		18	193	
Queue Length 95th (ft)		#276	59		191	47	38	308		42	280	
Internal Link Dist (ft)		169			92			328			322	
Turn Bay Length (ft)			100				185			155		

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WSL	WBT	WBR	NBL	NBT	NER	SBL	SBT	SBR
Base Capacity (vph)		343	581		253	588	407	2068		379	2059	
Starvation Cap Reductn		0	0		0	0	0	0		0	0	
Spillback Cap Reductn		0	0		0	0	0	0		0	0	
Storage Cap Reductn		0	0		0	0	0	0		0	0	
Reduced v/c Ratio		0.72	0.26		0.64	0.25	0.16	0.45		0.20	0.42	

Intersection Summary

Area Type: Other
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.89
 Intersection Signal Delay: 23.7
 Intersection Capacity Utilization: 59.6%
 Analysis Period (min): 15
 Intersection LOS: C
 ICU Level of Service: B

95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage



HCM Unsignalized Intersection Capacity Analysis

10: Frontage Road & Mystic Trace

3/9/2015



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Volume (veh/h)	0	98	129	13	33	5
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	107	140	14	36	5
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	154				254	147
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	154				254	147
tC, single (s)	4.1				6.4	6.2
tC, 2 stage (s)						
tF (s)	2.2				3.5	3.3
p0 queue free %	100				95	99
cM capacity (veh/h)	1426				735	900

Direction, Lane #	EB 1	WB 1	SB 1
Volume Total	107	154	41
Volume Left	0	0	36
Volume Right	0	14	5
cSH	1426	1700	753
Volume to Capacity	0.00	0.09	0.05
Queue Length 95th (ft)	0	0	4
Control Delay (s)	0.0	0.0	10.1
Lane LOS			B
Approach Delay (s)	0.0	0.0	10.1
Approach LOS			B

Intersection Summary			
Average Delay		1.4	
Intersection Capacity Utilization		17.6%	ICU Level of Service
Analysis Period (min)		15	A

Intersection Capacity Utilization

5: Frontage Road & Hinswood/Frontage/Hinswood Drive

3/9/2015



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	36	129	6	118	168	4
Pedestrians						
Ped Button						
Pedestrian Timing (s)						
Free Right		No				No
Ideal Flow	1900	1900	1900	1900	1900	1900
Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Green (s)	4.0	4.0	4.0	4.0	4.0	4.0
Refr Cycle Length (s)	120	120	120	120	120	120
Volume Combined (vph)	165	0	0	124	172	0
Lane Utilization Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Factor (vph)	0.88	0.85	0.95	1.00	0.95	0.85
Saturated Flow (vph)	1677	0	0	1895	1801	0
Ped Intf Time (s)	0.0	0.0	0.0	0.0	0.0	0.0
Pedestrian Frequency (%)	0.00			0.00	0.00	
Protected Option Allowed	No			No	No	
Reference Time (s)		0.0				0.0
Adj Reference Time (s)		0.0				0.0
Permitted Option						
Adj Saturation A (vph)	1677		0	1117	120	
Reference Time A (s)	11.8		0.0	13.3	171.9	
Adj Saturation B (vph)	1677		NA	NA	NA	
Reference Time B (s)	11.8		NA	NA	NA	
Reference Time (s)	11.8			13.3		
Adj Reference Time (s)	15.8			17.3		
Split Option						
Ref Time Combined (s)	11.8		0.0	7.9	11.5	
Ref Time Seperate (s)	2.6		0.4	7.5	11.2	
Reference Time (s)	11.8		7.9	7.9	11.5	
Adj Reference Time (s)	15.8		11.9	11.9	15.5	
Summary	EB WB		NB		Combined	
Protected Option (s)	NA		NA			
Permitted Option (s)	17.3		Err			
Split Option (s)	27.7		15.5			
Minimum (s)	17.3		15.5		32.8	

Right Turns
Adj Reference Time (s)
Cross Thru Ref Time (s)
Oncoming Left Ref Time (s)
Combined (s)

Intersection Summary			
Intersection Capacity Utilization	27.3%	ICU Level of Service	A
Reference Times and Phasing Options do not represent an optimized timing plan.			

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	91	59	74	56	96	93	218	730	71	136	764	110
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		100	0		0	185		0	155		0
Storage Lanes	0		1	0		1	1		0	1		0
Taper Length (ft)	25			25			160			170		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt			0.850				0.850	0.987			0.981	
Flt Protected		0.971			0.982		0.950			0.950		
Satd Flow (prot)	0	1809	1583	0	1829	1583	1770	3493	0	1770	3472	0
Flt Permitted		0.551			0.643		0.249			0.299		
Satd Flow (perm)	0	1026	1583	0	1198	1583	464	3493	0	557	3472	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)			79			99		12			19	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		249			172			408			402	
Travel Time (s)		5.7			3.9			9.3			9.1	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	160	79	0	162	99	232	853	0	145	930	0
Turn Type	Perm	NA	Perm	Perm	NA	Perm	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4		4	8		8	2			6		
Detector Phase	4	4	4	8	8	8	5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	
Minimum Split (s)	21.0	21.0	21.0	21.0	21.0	21.0	9.0	21.0		9.0	21.0	
Total Split (s)	42.0	42.0	42.0	42.0	42.0	42.0	13.0	65.0		13.0	65.0	
Total Split (%)	35.0%	35.0%	35.0%	35.0%	35.0%	35.0%	10.8%	54.2%		10.8%	54.2%	
Yellow Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)		5.0	5.0		5.0	5.0	5.0	5.0		5.0	5.0	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None	None	None	None	None	None	C-Max		None	C-Max	
Act Effct Green (s)		20.4	20.4		20.4	20.4	86.9	76.2		82.3	73.9	
Actuated g/C Ratio		0.17	0.17		0.17	0.17	0.72	0.64		0.69	0.62	
v/c Ratio		0.92	0.24		0.80	0.28	0.51	0.38		0.31	0.43	
Control Delay		98.5	9.9		74.1	9.5	9.4	12.1		7.2	13.8	
Queue Delay		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		98.5	9.9		74.1	9.5	9.4	12.1		7.2	13.8	
LOS		F	A		E	A	A	B		A	B	
Approach Delay		69.2			49.6			11.5			12.9	
Approach LOS		E			D			B			B	
Queue Length 50th (ft)		123	0		122	0	45	152		27	180	
Queue Length 95th (ft)		#196	40		187	44	93	247		59	295	
Internal Link Dist (ft)		169			92			328			322	
Turn Bay Length (ft)			100				185			155		

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Base Capacity (vph)		316	542		369	556	454	2222		474	2146	
Starvation Cap Reductn		0	0		0	0	0	0		0	0	
Spillback Cap Reductn		0	0		0	0	0	0		0	0	
Storage Cap Reductn		0	0		0	0	0	0		0	0	
Reduced v/c Ratio		0.51	0.15		0.44	0.18	0.51	0.38		0.31	0.43	

Intersection Summary

Area Type: Other
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.92
 Intersection Signal Delay: 21.0
 Intersection LOS: C
 Intersection Capacity Utilization: 64.0%
 ICU Level of Service: C
 Analysis Period (min): 15
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage



HCM Unsignalized Intersection Capacity Analysis

10: Frontage Road & Mystic Trace

3/9/2015



Movement	EBL	EBT	WBT	WSR	SBL	SBR
Lane Configurations						
Volume (veh/h)	5	126	154	36	15	3
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	137	167	39	16	3
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	207				335	187
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	207				335	187
tC, single (s)	4.1				6.4	6.2
tC, 2 stage (s)						
tF (s)	2.2				3.5	3.3
p0 queue free %	100				98	100
cM capacity (veh/h)	1365				658	855
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	142	207	20			
Volume Left	5	0	16			
Volume Right	0	39	3			
cSH	1365	1700	684			
Volume to Capacity	0.00	0.12	0.03			
Queue Length 95th (ft)	0	0	2			
Control Delay (s)	0.3	0.0	10.4			
Lane LOS	A		B			
Approach Delay (s)	0.3	0.0	10.4			
Approach LOS			B			
Intersection Summary						
Average Delay			0.7			
Intersection Capacity Utilization			20.7%	ICU Level of Service		A
Analysis Period (min)			15			

Intersection Capacity Utilization

5: Frontage Road & Hinswood/Frontage/Hinswood Drive

3/9/2015



Movement	EBT	ESR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	94	172	2	53	192	3
Pedestrians						
Ped Button						
Pedestrian Timing (s)						
Free Right		No				No
Ideal Flow	1900	1900	1900	1900	1900	1900
Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Green (s)	4.0	4.0	4.0	4.0	4.0	4.0
Refr Cycle Length (s)	120	120	120	120	120	120
Volume Combined (vph)	266	0	0	55	195	0
Lane Utilization Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Factor (vph)	0.90	0.85	0.95	1.00	0.95	0.85
Saturated Flow (vph)	1716	0	0	1897	1802	0
Ped Intf Time (s)	0.0	0.0	0.0	0.0	0.0	0.0
Pedestrian Frequency (%)	0.00			0.00	0.00	
Protected Option Allowed	No			No	No	
Reference Time (s)		0.0				0.0
Adj Reference Time (s)		0.0				0.0
Permitted Option						
Adj Saturation A (vph)	1716		0	1248	120	
Reference Time A (s)	18.6		0.0	5.3	194.8	
Adj Saturation B (vph)	1716		NA	NA	NA	
Reference Time B (s)	18.6		NA	NA	NA	
Reference Time (s)	18.6			5.3		
Adj Reference Time (s)	22.6			9.3		
Split Option						
Ref Time Combined (s)	18.6		0.0	3.5	13.0	
Ref Time Seperate (s)	6.6		0.1	3.3	12.8	
Reference Time (s)	18.6		3.5	3.5	13.0	
Adj Reference Time (s)	22.6		8.0	8.0	17.0	
Summary						
	EB	WB	NB	Combined		
Protected Option (s)	NA		NA			
Permitted Option (s)	22.6		Err			
Split Option (s)	30.6		17.0			
Minimum (s)	22.6		17.0	39.6		

Right Turns	
Adj Reference Time (s)	
Cross Thru Ref Time (s)	
Oncoming Left Ref Time (s)	
Combined (s)	

Intersection Summary			
Intersection Capacity Utilization	33.0%	ICU Level of Service	A
Reference Times and Phasing Options do not represent an optimized timing plan.			

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	170	73	148	122	50	151	63	870	48	88	772	67
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		100	0		0	185		0	155		0
Storage Lanes	0		1	0		1	1		0	1		0
Taper Length (ft)	25			25			160			170		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frt			0.850			0.850		0.992			0.988	
Flt Protected		0.966			0.966		0.950			0.950		
Satd. Flow (prot)	0	1799	1583	0	1799	1583	1770	3511	0	1770	3497	0
Flt Permitted		0.571			0.438		0.256			0.210		
Satd. Flow (perm)	0	1064	1583	0	816	1583	477	3511	0	391	3497	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)			132			162		7			11	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		249			172			408			402	
Travel Time (s)		5.7			3.9			9.3			9.1	
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	261	159	0	185	162	68	987	0	95	902	0
Turn Type	Perm	NA	Perm	Perm	NA	Perm	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4		4	8		8	2			6		
Detector Phase	4	4	4	8	8	8	5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	
Minimum Split (s)	21.0	21.0	21.0	21.0	21.0	21.0	9.0	21.0		9.0	21.0	
Total Split (s)	42.0	42.0	42.0	42.0	42.0	42.0	13.0	65.0		13.0	65.0	
Total Split (%)	35.0%	35.0%	35.0%	35.0%	35.0%	35.0%	10.8%	54.2%		10.8%	54.2%	
Yellow Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)		5.0	5.0		5.0	5.0	5.0	5.0		5.0	5.0	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None	None	None	None	None	None	C-Max		None	C-Max	
Act Effct Green (s)		31.9	31.9		31.9	31.9	72.7	65.7		74.3	68.1	
Actuated g/C Ratio		0.27	0.27		0.27	0.27	0.61	0.55		0.62	0.57	
v/c Ratio		0.92	0.31		0.86	0.30	0.19	0.51		0.29	0.45	
Control Delay		79.4	9.5		74.3	6.1	10.2	19.1		11.3	17.4	
Queue Delay		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		79.4	9.5		74.3	6.1	10.2	19.1		11.3	17.4	
LOS		E	A		E	A	B	B		B	B	
Approach Delay		52.9			42.5			18.6			16.9	
Approach LOS		D			D			B			B	
Queue Length 50th (ft)		192	15		133	0	19	254		27	220	
Queue Length 95th (ft)		#326	66		#244	49	39	332		52	295	
Internal Link Dist (ft)		169			92			328			322	
Turn Bay Length (ft)			100				185			155		

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

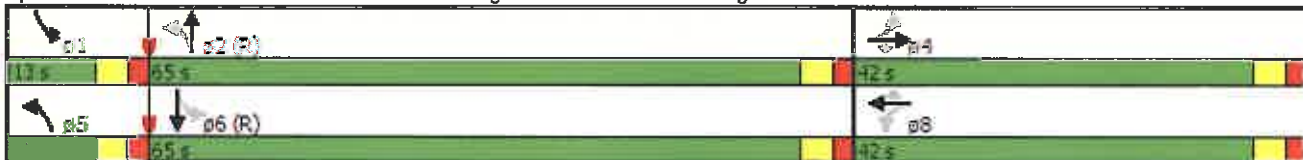
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Base Capacity (vph)		328	579		251	600	379	1925		334	1990	
Starvation Cap Reductn		0	0		0	0	0	0		0	0	
Spillback Cap Reductn		0	0		0	0	0	0		0	0	
Storage Cap Reductn		0	0		0	0	0	0		0	0	
Reduced v/c Ratio		0.80	0.27		0.74	0.27	0.18	0.51		0.28	0.45	

Intersection Summary

Area Type: Other
 Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.92
 Intersection Signal Delay: 26.0
 Intersection Capacity Utilization 62.9%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service B











95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage



HCM Unsignalized Intersection Capacity Analysis
 9: Access Drive & Frontage Road

3/9/2015

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (veh/h)	102	37	2	148	26	1
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	111	40	2	161	28	1
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume			151		296	131
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			151		296	131
tC, single (s)			4.1		6.4	6.2
tC, 2 stage (s)						
tF (s)			2.2		3.5	3.3
p0 queue free %			100		96	100
cM capacity (veh/h)			1430		694	919
Direction, Lane #	EB 1	WB 1	NB 1			
Volume Total	151	163	29			
Volume Left	0	2	28			
Volume Right	40	0	1			
cSH	1700	1430	700			
Volume to Capacity	0.09	0.00	0.04			
Queue Length 95th (ft)	0	0	3			
Control Delay (s)	0.0	0.1	10.4			
Lane LOS		A	B			
Approach Delay (s)	0.0	0.1	10.4			
Approach LOS			B			
Intersection Summary						
Average Delay			0.9			
Intersection Capacity Utilization			19.4%	ICU Level of Service		A
Analysis Period (min)			15			

HCM Unsignalized Intersection Capacity Analysis

10: Frontage Road & Mystic Trace

3/9/2015



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Volume (veh/h)	0	139	160	14	34	5
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	151	174	15	37	5
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	189				333	182
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	189				333	182
tC, single (s)	4.1				6.4	6.2
tC, 2 stage (s)						
tF (s)	2.2				3.5	3.3
p0 queue free %	100				94	99
cM capacity (veh/h)	1385				662	861

Direction, Lane #	EB 1	WB 1	SB 1
Volume Total	151	189	42
Volume Left	0	0	37
Volume Right	0	15	5
cSH	1385	1700	683
Volume to Capacity	0.00	0.11	0.06
Queue Length 95th (ft)	0	0	5
Control Delay (s)	0.0	0.0	10.6
Lane LOS			B
Approach Delay (s)	0.0	0.0	10.6
Approach LOS			B

Intersection Summary			
Average Delay		1.2	
Intersection Capacity Utilization		19.3%	ICU Level of Service A
Analysis Period (min)		15	

Intersection Capacity Utilization

5: Frontage Road & Hinswood/Frontage/Hinswood Drive

3/9/2015



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	→	↘	↙	←	↖	↗
Volume (vph)	37	171	6	123	201	4
Pedestrians						
Ped Button						
Pedestrian Timing (s)						
Free Right		No				No
Ideal Flow	1900	1900	1900	1900	1900	1900
Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Green (s)	4.0	4.0	4.0	4.0	4.0	4.0
Refr Cycle Length (s)	120	120	120	120	120	120
Volume Combined (vph)	208	0	0	129	205	0
Lane Utilization Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Factor (vph)	0.88	0.85	0.95	1.00	0.95	0.85
Saturated Flow (vph)	1666	0	0	1896	1802	0
Ped Intf Time (s)	0.0	0.0	0.0	0.0	0.0	0.0
Pedestrian Frequency (%)	0.00			0.00	0.00	
Protected Option Allowed	No			No	No	
Reference Time (s)		0.0				0.0
Adj Reference Time (s)		0.0				0.0
Permitted Option						
Adj Saturation A (vph)	1666		0	1136	120	
Reference Time A (s)	15.0		0.0	13.6	204.8	
Adj Saturation B (vph)	NA		NA	NA	NA	
Reference Time B (s)	NA		NA	NA	NA	
Reference Time (s)	15.0			13.6		
Adj Reference Time (s)	19.0			17.6		
Split Option						
Ref Time Combined (s)	15.0		0.0	8.2	13.7	
Ref Time Seperate (s)	2.7		0.4	7.8	13.4	
Reference Time (s)	15.0		8.2	8.2	13.7	
Adj Reference Time (s)	19.0		12.2	12.2	17.7	
Summary						
	EB	WB	NB	Combined		
Protected Option (s)	NA		NA			
Permitted Option (s)	19.0		Err			
Split Option (s)	31.2		17.7			
Minimum (s)	19.0		17.7	36.6		

Right Turns

Adj Reference Time (s)	
Cross Thru Ref Time (s)	
Oncoming Left Ref Time (s)	
Combined (s)	

Intersection Summary

Intersection Capacity Utilization	30.5%	ICU Level of Service	A
Reference Times and Phasing Options do not represent an optimized timing plan.			

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	95	65	77	74	104	111	227	759	91	156	795	114
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	0		100	0		0	185		0	155		0
Storage Lanes	0		1	0		1	1		0	1		0
Taper Length (ft)	25			25			160			170		
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.95	0.95	1.00	0.95	0.95
Frnt			0.850			0.850		0.984			0.981	
Fit Protected		0.971			0.980		0.950			0.950		
Satd. Flow (prot)	0	1809	1583	0	1825	1583	1770	3483	0	1770	3472	0
Fit Permitted		0.505			0.606		0.231			0.273		
Satd. Flow (perm)	0	941	1583	0	1129	1583	430	3483	0	509	3472	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)			82			118		15			19	
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		249			172			408			402	
Travel Time (s)		5.7			3.9			9.3			9.1	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	170	82	0	190	118	241	904	0	166	967	0
Turn Type	Perm	NA	Perm	Perm	NA	Perm	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4		4	8		8	2			6		
Detector Phase	4	4	4	8	8	8	5	2		1	6	
Switch Phase												
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	
Minimum Split (s)	21.0	21.0	21.0	21.0	21.0	21.0	9.0	21.0		9.0	21.0	
Total Split (s)	42.0	42.0	42.0	42.0	42.0	42.0	13.0	65.0		13.0	65.0	
Total Split (%)	35.0%	35.0%	35.0%	35.0%	35.0%	35.0%	10.8%	54.2%		10.8%	54.2%	
Yellow Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)		5.0	5.0		5.0	5.0	5.0	5.0		5.0	5.0	
Lead/Lag							Lead	Lag		Lead	Lag	
Lead-Lag Optimize?							Yes	Yes		Yes	Yes	
Recall Mode	None	None	None	None	None	None	None	C-Max		None	C-Max	
Act Effect Green (s)		22.4	22.4		22.4	22.4	84.7	73.5		80.5	71.4	
Actuated g/C Ratio		0.19	0.19		0.19	0.19	0.71	0.61		0.67	0.60	
v/c Ratio		0.97	0.23		0.90	0.30	0.56	0.42		0.38	0.47	
Control Delay		107.9	9.1		87.8	8.4	11.5	14.0		8.7	15.6	
Queue Delay		0.0	0.0		0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		107.9	9.1		87.8	8.4	11.5	14.0		8.7	15.6	
LOS		F	A		F	A	B	B		A	B	
Approach Delay		75.7			57.4			13.5			14.6	
Approach LOS		E			E			B			B	
Queue Length 50th (ft)		132	0		145	0	51	176		34	205	
Queue Length 95th (ft)		#217	39		217	46	105	289		73	321	
Internal Link Dist (ft)		169			92			328			322	
Turn Bay Length (ft)			100				185			155		

Lanes, Volumes, Timings

3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage

3/9/2015

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Base Capacity (vph)		290	544		348	569	428	2137		442	2072	
Starvation Cap Reductn		0	0		0	0	0	0		0	0	
Spillback Cap Reductn		0	0		0	0	0	0		0	0	
Storage Cap Reductn		0	0		0	0	0	0		0	0	
Reduced v/c Ratio		0.59	0.15		0.55	0.21	0.56	0.42		0.38	0.47	

Intersection Summary

Area Type: Other

Cycle Length: 120

Actuated Cycle Length: 120

Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.97

Intersection Signal Delay: 24.2

Intersection LOS: C

Intersection Capacity Utilization 66.9%

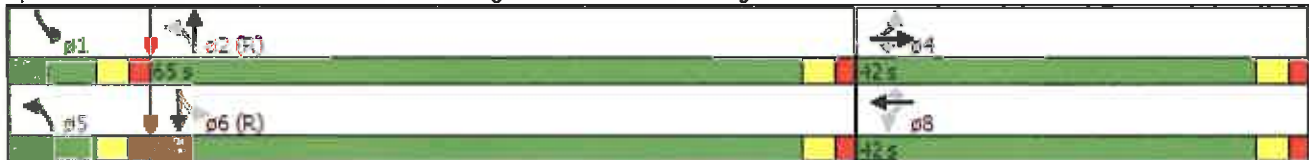
ICU Level of Service C

Analysis Period (min) 15

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Cass Avenue & I-55 Frontage Road/Hinswood/Frontage



HCM Unsignalized Intersection Capacity Analysis

10: Frontage Road & Mystic Trace

3/9/2015












Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Volume (veh/h)	5	167	194	37	16	3
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	182	211	40	17	3
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	251				423	231
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	251				423	231
tC, single (s)	4.1				6.4	6.2
tC, 2 stage (s)						
tF (s)	2.2				3.5	3.3
p0 queue free %	100				97	100
cM capacity (veh/h)	1314				585	808

Direction, Lane #	EB 1	WB 1	SB 1
Volume Total	187	251	21
Volume Left	5	0	17
Volume Right	0	40	3
cSH	1314	1700	612
Volume to Capacity	0.00	0.15	0.03
Queue Length 95th (ft)	0	0	3
Control Delay (s)	0.3	0.0	11.1
Lane LOS	A		B
Approach Delay (s)	0.3	0.0	11.1
Approach LOS			B

Intersection Summary			
Average Delay		0.6	
Intersection Capacity Utilization	22.8%	ICU Level of Service	A
Analysis Period (min)	15		

HCM Unsignalized Intersection Capacity Analysis
 12: Access Drive & Frontage Road

3/9/2015

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (veh/h)	136	36	2	198	34	2
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	148	39	2	215	37	2
Pedestrians						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume			187		387	167
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			187		387	167
tC, single (s)			4.1		6.4	6.2
tC, 2 stage (s)						
tF (s)			2.2		3.5	3.3
p0 queue free %			100		94	100
cM capacity (veh/h)			1387		615	877
Direction, Lane #	EB 1	WB 1	NB 1			
Volume Total	187	217	39			
Volume Left	0	2	37			
Volume Right	39	0	2			
cSH	1700	1387	626			
Volume to Capacity	0.11	0.00	0.06			
Queue Length 95th (ft)	0	0	5			
Control Delay (s)	0.0	0.1	11.1			
Lane LOS		A	B			
Approach Delay (s)	0.0	0.1	11.1			
Approach LOS			B			
Intersection Summary						
Average Delay			1.0			
Intersection Capacity Utilization			22.0%	ICU Level of Service		A
Analysis Period (min)			15			

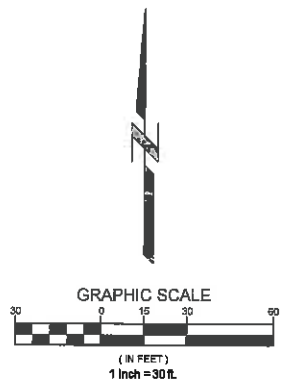
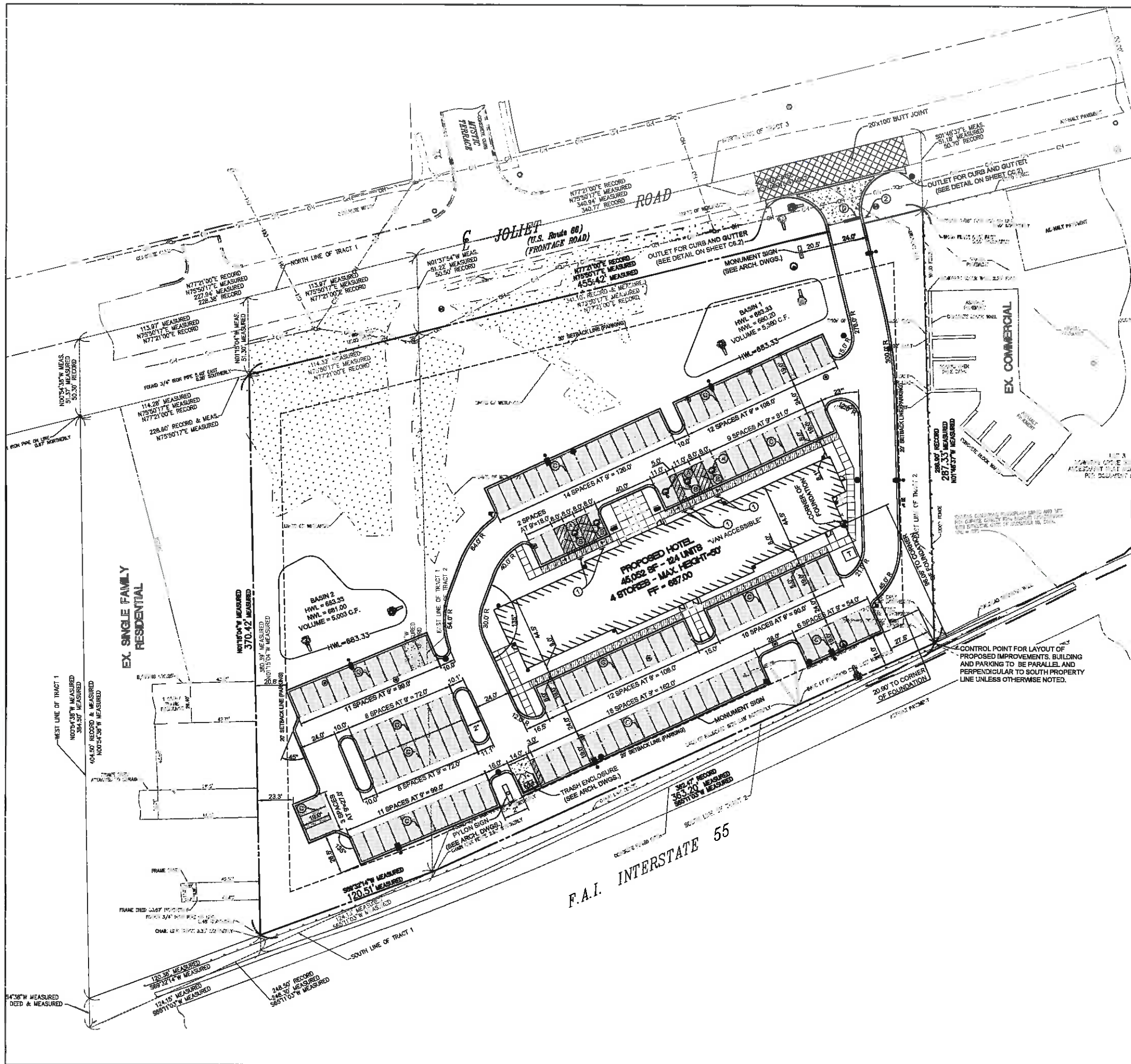
Intersection Capacity Utilization

5: Frontage Road & Hinswood/Frontage/Hinswood Drive

3/9/2015



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	98	215	2	55	234	3
Pedestrians						
Ped Button						
Pedestrian Timing (s)						
Free Right		No				No
Ideal Flow	1900	1900	1900	1900	1900	1900
Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Green (s)	4.0	4.0	4.0	4.0	4.0	4.0
Refr Cycle Length (s)	120	120	120	120	120	120
Volume Combined (vph)	313	0	0	57	237	0
Lane Utilization Factor	1.00	1.00	1.00	1.00	1.00	1.00
Turning Factor (vph)	0.90	0.85	0.95	1.00	0.95	0.85
Saturated Flow (vph)	1704	0	0	1897	1803	0
Ped Intf Time (s)	0.0	0.0	0.0	0.0	0.0	0.0
Pedestrian Frequency (%)	0.00			0.00	0.00	
Protected Option Allowed	No			No	No	
Reference Time (s)		0.0				0.0
Adj Reference Time (s)		0.0				0.0
Permitted Option						
Adj Saturation A (vph)	1704		0	1263	120	
Reference Time A (s)	22.0		0.0	5.4	236.6	
Adj Saturation B (vph)	1704		NA	NA	NA	
Reference Time B (s)	22.0		NA	NA	NA	
Reference Time (s)	22.0			5.4		
Adj Reference Time (s)	26.0			9.4		
Split Option						
Ref Time Combined (s)	22.0		0.0	3.6	15.8	
Ref Time Seperate (s)	6.9		0.1	3.5	15.6	
Reference Time (s)	22.0		3.6	3.6	15.8	
Adj Reference Time (s)	26.0		8.0	8.0	19.8	
Summary	EB WB		NB		Combined	
Protected Option (s)	NA		NA			
Permitted Option (s)	26.0		En			
Split Option (s)	34.0		19.8			
Minimum (s)	26.0		19.8		45.8	
Right Turns						
Adj Reference Time (s)						
Cross Thru Ref Time (s)						
Oncoming Left Ref Time (s)						
Combined (s)						
Intersection Summary						
Intersection Capacity Utilization			38.2%		ICU Level of Service	A
Reference Times and Phasing Options do not represent an optimized timing plan.						



SITE DATA	
TOTAL SITE AREA =	3.40± AC
TOTAL BUILDING AREA =	45,052 G.S.F.
PARKING REQUIRED:	
1 SPACE PER UNIT =	124 SPACES
1 SPACE PER EMPLOYEE =	5 SPACES
TOTAL PARKING REQUIRED =	129 SPACES
PARKING PROVIDED:	
REGULAR PARKING PROVIDED =	124 SPACES
HANDICAP PARKING PROVIDED =	5 SPACES
TOTAL PARKING PROVIDED =	129 SPACES

PAVEMENT MARKING AND SIGN LEGEND	
①	R7-8 HANDICAP PARKING SIGN (12'X18') WITH \$250 FINE AND R7-8A VAN ACCESSIBLE SIGN WHERE NOTED
②	R1-1 STOP SIGN (30'X30')
Ⓐ	HANDICAP SYMBOL PER LATEST ADA STANDARDS
Ⓑ	4" SOLID YELLOW AT 24° C-C AT 45 DEGREES
Ⓒ	4" SOLID YELLOW STRIPE (TYP)
Ⓓ	24" WIDE WHITE STOP BAR

- GENERAL NOTES**
- ALL RADII ARE 5.0' UNLESS OTHERWISE NOTED.
 - CURB SHALL BE 86.12 CURB AND GUTTER UNLESS OTHERWISE NOTED.
 - ALL DIMENSIONS AND CURB RADII ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL SITE SIGNAGE SHALL BE IN CONFORMANCE WITH M.U.T.C.D. STANDARDS, LATEST EDITION.
 - ALL PARKING SPACES SHALL BE ANGLED AT 90° TO THE ASSOCIATED DRIVE AISLE.
 - DETECTABLE WARNING STRIPS WITH TRUNCATED DOMES ON ALL CURB RAMPS SHALL CONSIST OF CONTRASTING COLOR TO ADJACENT PAVEMENT COLOR PER IDOT REGULATIONS, PREFERRED COLOR IS RED (FEDERAL COLOR STANDARD 30168).
 - REFER TO ARCHITECTURAL AND SIGNAGE PLANS FOR ANY ADDITIONAL SITE SIGNAGE AND PAVEMENT MARKINGS.
 - ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM OF SIGN.
 - IF THE FRONTAGE ROAD PAVEMENT IS DAMAGED DURING CONSTRUCTION, THE PAVEMENT SHALL BE SAW CUT (PARTIAL DEPTH) A MINIMUM OF 2" FROM THE EDGE OF THE NEW CURB/GUTTER FLAG AND REPLACED WITH LIKE MATERIALS (SURFACE COURSE ONLY).
 - ALL UNDERGROUND UTILITIES ARE TO BE INSTALLED PRIOR TO BEGINNING PAVEMENT CONSTRUCTION.
 - REFER TO ARCHITECTURAL DRAWINGS FOR LIGHT POLE AND MONUMENT SIGN DESIGN, LOCATION DIMENSIONS, AND CONSTRUCTION DETAILS.

SITE PAVING LEGEND	
	HEAVY DUTY ASPHALT PAVEMENT 1.5" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, NSD 12" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	LIGHT DUTY ASPHALT PAVEMENT 1.5" BITUMINOUS SURFACE COURSE, HOT MIX ASPHALT, MIX D, NSD 2.5" BITUMINOUS BINDER COURSE, HOT MIX ASPHALT, IL-19, NSD 11" AGGREGATE BASE COURSE, TYPE B (DOT GRAD CA-6)
	CONCRETE PAVEMENT 8" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	SIDEWALK PAVEMENT 5" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)
	COMBINATION SIDEWALK AND CURB AND GUTTER PAVEMENT 5" P.C. CONCRETE PAVEMENT 4" AGGREGATE BASE (CA-6)

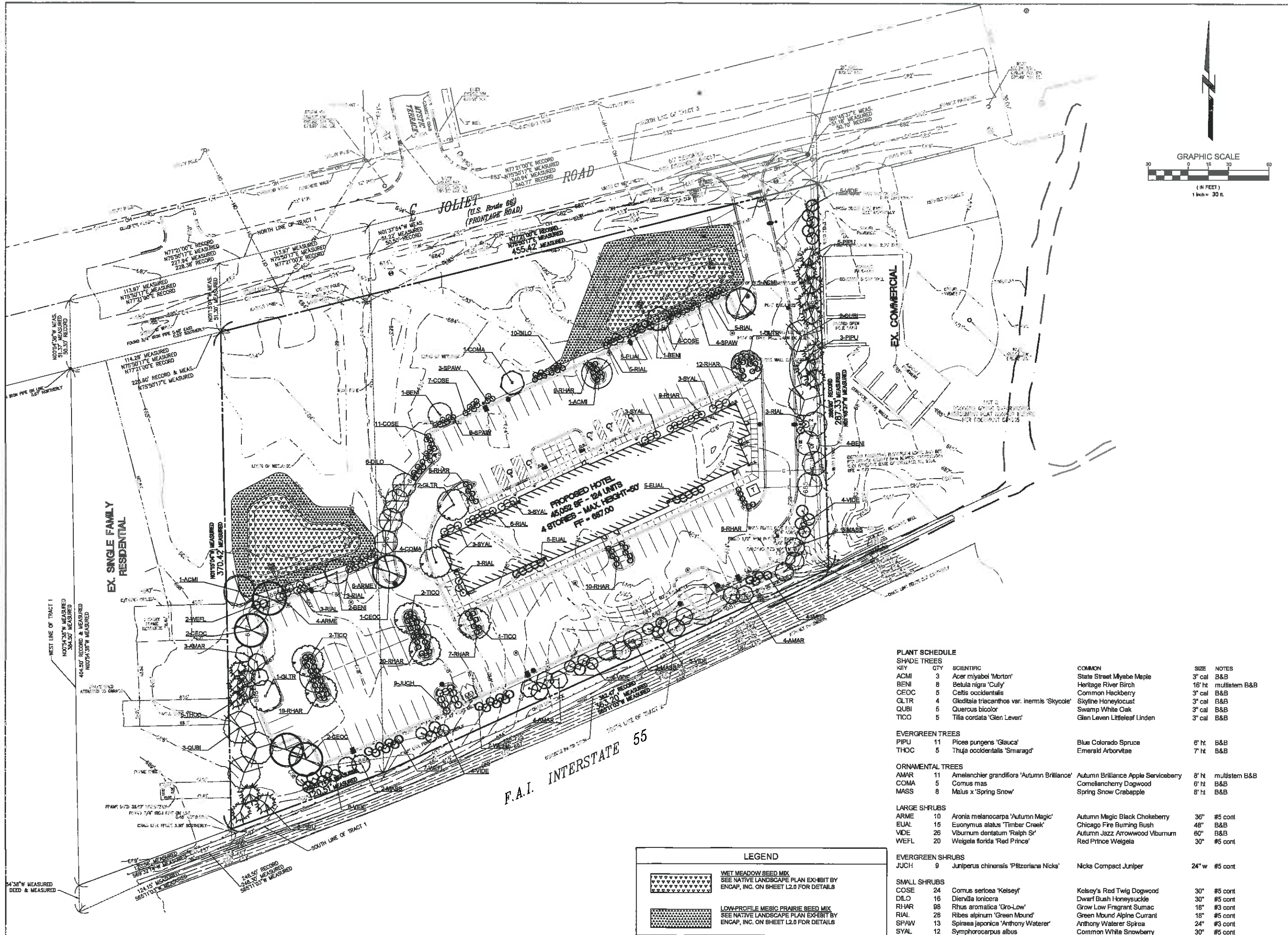
CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS
CIVWORKS
 Consulting, LLC
 2800 N. DEWITT AVE.
 CHICAGO, ILLINOIS 60642
 PH: (312) 637-9270
 FAX: (312) 637-9454
 E-MAIL: info@civworks.com
 WWW: www.civworks.com

REVISIONS

NO. 1	DATE	DESCRIPTION

SITE DIMENSIONAL AND PAVING PLAN
 PROPOSED VALUE PLACE HOTEL
 951 N. FRONTAGE ROAD, DARIEN, IL
 REVIEW SET - PENDING APPROVAL - NOT FOR CONSTRUCTION

PROJ. MGR.: OP
 PROJ. ENG.: OP
 DRAWN BY:
 DATE: 04-03-2015
 SCALE: 1"=30'
 SHEET NO.
C3.0
 PROJ. NUMBER: 14013



NO.	DATE	REVISIONS
1	05-22-15	REVISED PER CITY REVIEW

CIVIL ENGINEERS - PLANNERS - DEVELOPMENT CONSULTANTS

CIVWORKS
Consulting, LLC

2045 N. NAVA AVE
CHICAGO, ILLINOIS 60644
PH: (312) 637-9070
FAX: (312) 637-9454
E-mail: info@civworks.com
Web: www.civworks.com

SITE LANDSCAPING PLAN

PROPOSED VALUE PLACE HOTEL

951 N. FRONTAGE ROAD, DARIEN, IL

REVIEW SET - PENDING APPROVAL - NOT FOR CONSTRUCTION

PROJ. MGR.: OF

PROJ. ENG.: OF

DRAWN BY:

DATE: 04-03-2015

SCALE: 1"=30'

SHEET NO.

L1.0

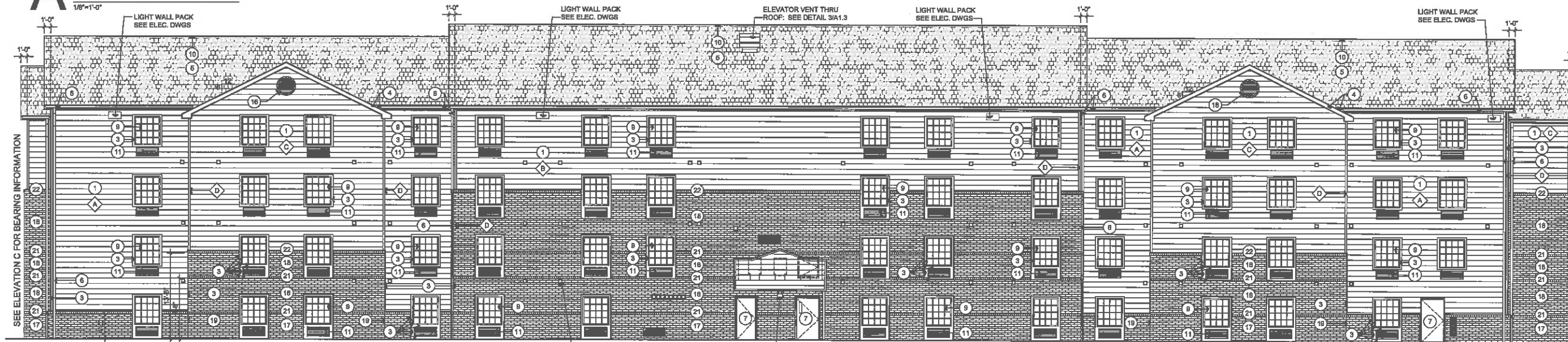
PROJ. NUMBER: 14013

PLANT SCHEDULE					
KEY	QTY	SCIENTIFIC	COMMON	SIZE	NOTES
SHADE TREES					
ACMI	3	Acer miyabei 'Morton'	State Street Myabe Maple	3" cal	B&B
BENI	8	Betula nigra 'Cully'	Heritage River Birch	16' ht	multistem B&B
CEOC	5	Celtis occidentalis	Common Hackberry	3" cal	B&B
GLTR	4	Gleditsia triacanthos var. inermis 'Skycoke'	Skyline Honeylocust	3" cal	B&B
QUBI	5	Quercus bicolor	Swamp White Oak	3" cal	B&B
TICO	5	Tilia cordata 'Glen Leven'	Glen Leven Littleleaf Linden	3" cal	B&B
EVERGREEN TREES					
PIPU	11	Picea pungens 'Glauca'	Blue Colorado Spruce	6' ht	B&B
THOC	5	Thuja occidentalis 'Smaragd'	Emerald Arborvitae	7' ht	B&B
ORNAMENTAL TREES					
AMAR	11	Amelanchier grandiflora 'Autumn Brilliance'	Autumn Brilliance Apple Serviceberry	8' ht	multistem B&B
COMA	5	Cornus mas	Cornelancherry Dogwood	8' ht	B&B
MASS	8	Malus x 'Spring Snow'	Spring Snow Crabapple	8' ht	B&B
LARGE SHRUBS					
ARME	10	Aronia melanocarpa 'Autumn Magic'	Autumn Magic Black Chokeberry	36"	#5 cont
EUAL	15	Euronymus alatus 'Timber Creek'	Chicago Fire Burning Bush	48"	B&B
VIDE	25	Viburnum dentatum 'Ralph S'	Autumn Jazz Arrowwood Viburnum	60"	B&B
WEFL	20	Weigela florida 'Red Prince'	Red Prince Weigela	30"	#5 cont
EVERGREEN SHRUBS					
JUCH	9	Juniperus chinensis 'Pfitzeriana Nicks'	Nicks Compact Juniper	24" w	#5 cont
SMALL SHRUBS					
COSE	24	Cornus sericea 'Kelsey'	Kelsey's Red Twig Dogwood	30"	#5 cont
DILO	16	Diervilla lonicera	Dwarf Bush Honeysuckle	30"	#5 cont
RHAR	98	Rhus aromatica 'Gro-Low'	Grow Low Fragrant Sumac	18"	#3 cont
RIAL	28	Ribes alpinum 'Green Mound'	Green Mound Alpine Currant	18"	#5 cont
SPAW	13	Spiraea japonica 'Anthony Waterer'	Anthony Waterer Spirea	24"	#5 cont
SYAL	12	Symphoricarpos albus	Common White Snowberry	30"	#5 cont

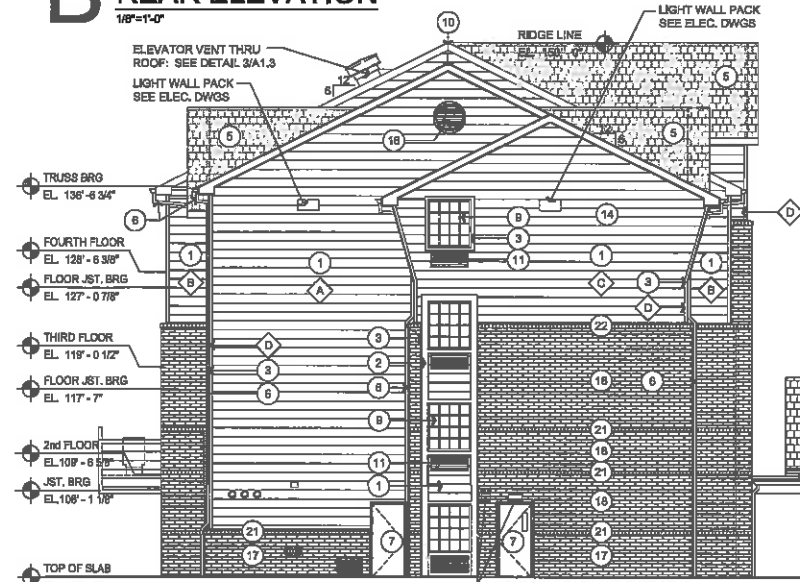
LEGEND	
	WET MEADOW SEED MIX SEE NATIVE LANDSCAPE PLAN EXHIBIT BY ENCAP, INC. ON SHEET L2.0 FOR DETAILS
	LOW-PROFILE MESIC PRAIRIE SEED MIX SEE NATIVE LANDSCAPE PLAN EXHIBIT BY ENCAP, INC. ON SHEET L2.0 FOR DETAILS



A FRONT ELEVATION
1/8"=1'-0"

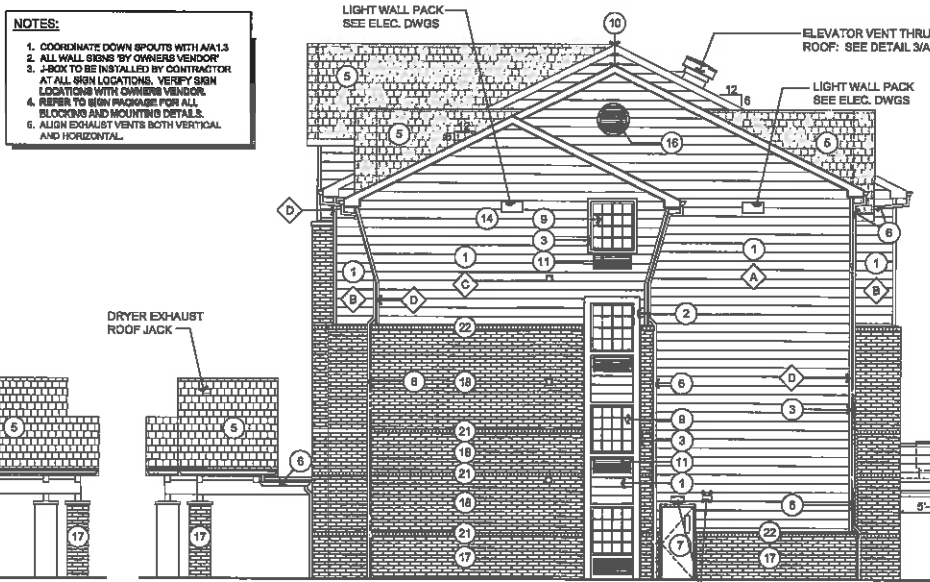


B REAR ELEVATION
1/8"=1'-0"



C LEFT SIDE ELEVATION
1/8"=1'-0"

NOTES:
1. COORDINATE DOWN SPOULTS WITH A/A1.3
2. ALL WALL SIGNS BY OWNERS VENDOR
3. J-BOX TO BE INSTALLED BY CONTRACTOR AT ALL SIGN LOCATIONS. VERIFY SIGN LOCATIONS WITH OWNERS VENDOR.
4. REFER TO SIGN PACKAGE FOR ALL SLOCATIONS AND MOUNTING DETAILS.
5. ALIGN EXHAUST VENTS BOTH VERTICAL AND HORIZONTAL.

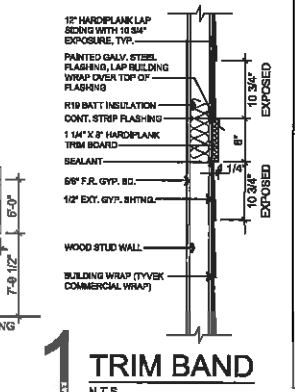


D RIGHT SIDE ELEVATION
1/8"=1'-0"

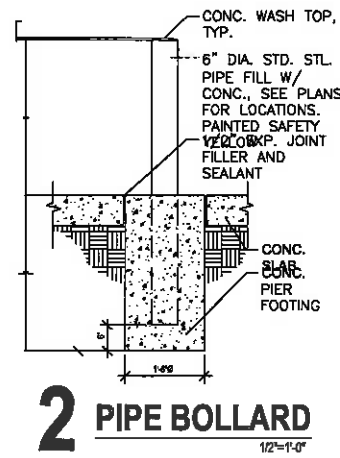
PAINT SCHEDULE

- 1. TRIM 1 1/2" SW7900
- 2. PRACTICAL BRICK SW8100
- 3. 1/2" X 1/2" SW8100
- 4. ALABASTER SW7000
- 5. TOTALLY TAN SW8116

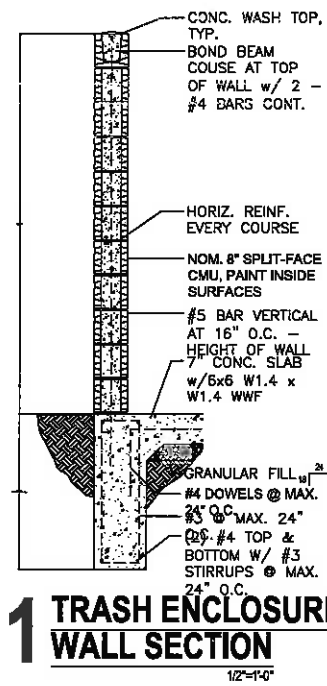
GENERAL NOTE
*ALL "HARDI" BOARD PRODUCTS ARE THE PREFABRICATED EXTERIOR CONCRETE SIDING. SUBSTITUTIONS ONLY WITH OWNERS WRITTEN AUTHORIZATION.



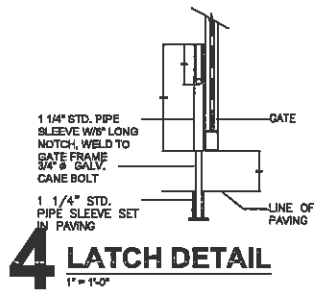
- ELEVATION MATERIALS**
1. WALLS: 10" EXPOSURE "HARDIPLANK" LAP SIDING
 2. 5/8" X 8" WIDE "HARDIPLANK" TRIM: SEE COLOR SCHEDULE. SEE DETAIL 1/A2.1 (NOTE: DIM. TO BTM. OF TRIM IS APPROX. - G.C. TO V.L.F. TO KEEP FROM CUTTING SIDING AS MUCH AS POSSIBLE)
 3. "HARDIPLANK" TRIM: SEE DETAILS FOR SIZES
 4. "HARDIPLANK" TRIM FASCIA
 5. ROOF: COMPOSITION SHINGLES
 6. 5" X 4" ALUMINUM GUTTERS AND DOWN SPOULTS: SEE COLOR SCHEDULE
 7. HOLLOW METAL DOORS PAINTED BRONZETONE. TRIM TO BE 1
 8. GLASS / ALUMINUM DOORS
 9. SLIDING WINDOW TYP.: SEE SPECS
 10. RIDGE VENT: SEE ROOF PLAN A/A1.3 & DETAIL 1/A1.3
 11. THRU-WALL HVAC UNIT
 12. MECH. LOUVERS: SEE MECH. DWGS. FOR SIZE, ALSO REF. STRUCT. DWGS. FOR HEADER SIZE AND LOCATION: PAINT LOUVER TO MATCH ADJACENT SURFACE
 13. KNOX BOX - VERIFY SIZE, LOCATION, AND TYPE WITH LOCAL FIRE DEPARTMENT
 14. COORDINATE SIGNAGE TYPE(S) AND LOCATION(S) WITH OWNER
 15. MOLDED PLASTIC SHUTTER: INTEGRAL COLOR - COLOR: BLACK
 16. 36" DIA. PLASTIC GABLE VENT, INTEGRAL COLOR W/ INSECT SCREEN: COLOR TO MATCH ADJACENT SURFACE: SEE SPECS
 17. UTILITY FACE BRICK, RUNNING BOND, SIOUX CITY "FINE ART VELOUR"
 18. UTILITY FACE BRICK, RUNNING BOND, SIOUX CITY "TOASTED FINE ART VELOUR"
 19. CONTROL JOINT IN BRICK
 20. NOT USED
 21. UTILITY FACE BRICK, ROWLOCK COURSE, SIOUX CITY "CLEAR BUFF VELOUR"
 22. UTILITY BRICK ROWLOCK CAP, SIOUX CITY "CLEAR BUFF VELOUR"



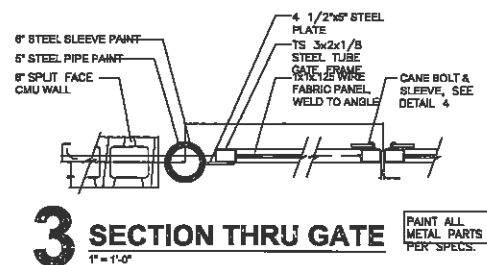
2 PIPE BOLLARD
1/2" = 1'-0"



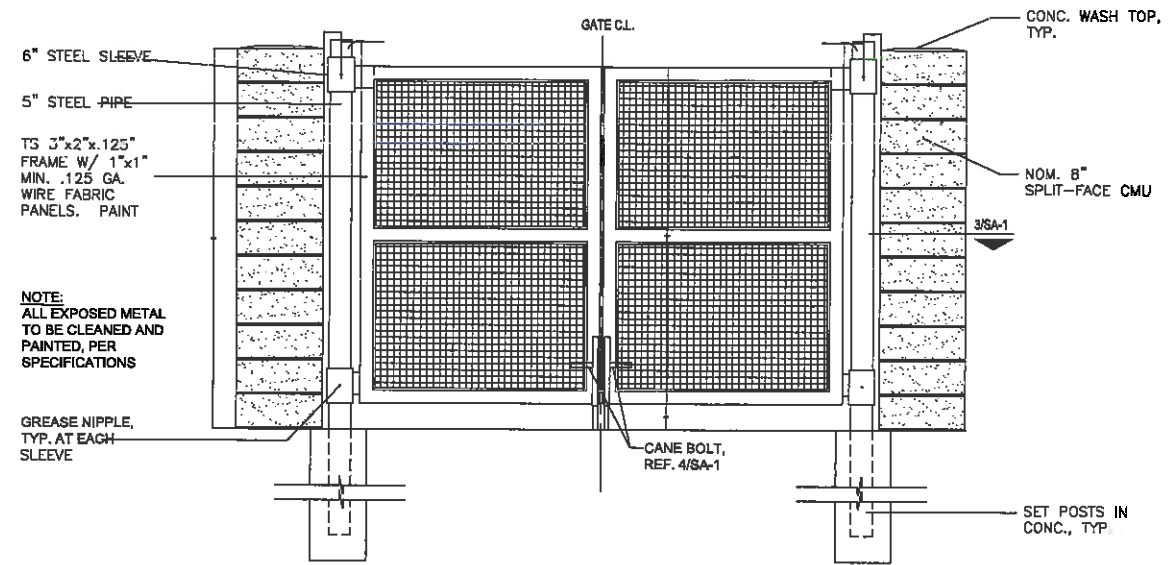
1 TRASH ENCLOSURE WALL SECTION
1/2" = 1'-0"



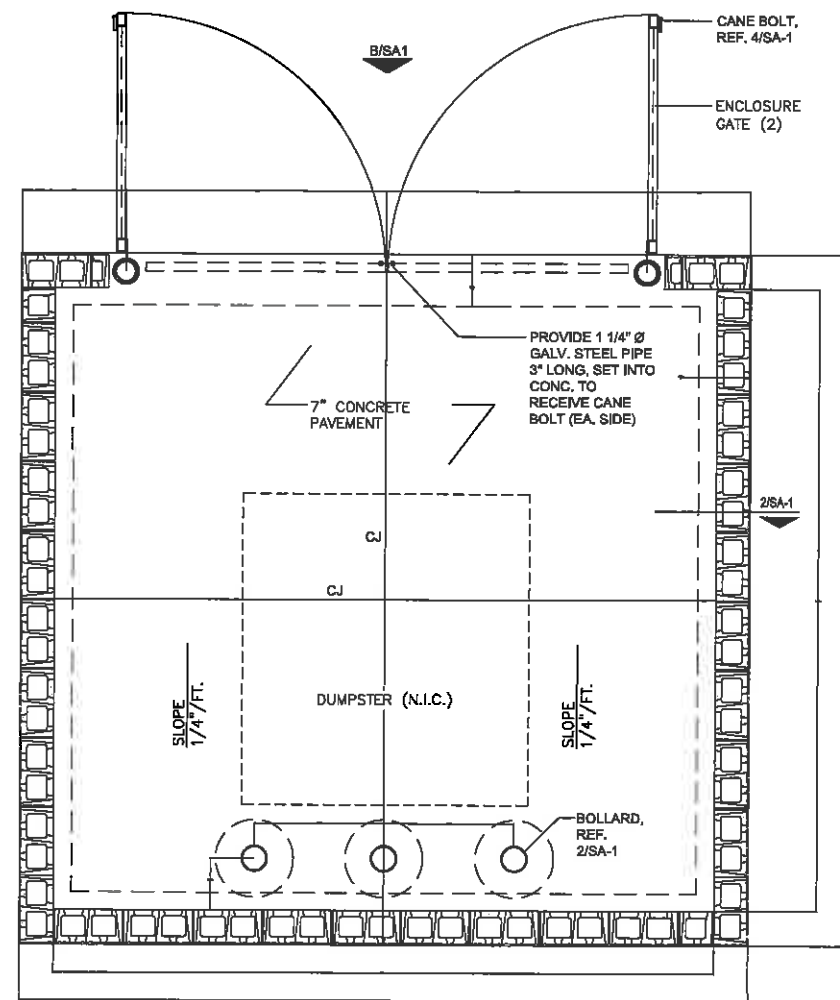
4 LATCH DETAIL
1" = 1'-0"



3 SECTION THRU GATE
1" = 1'-0"

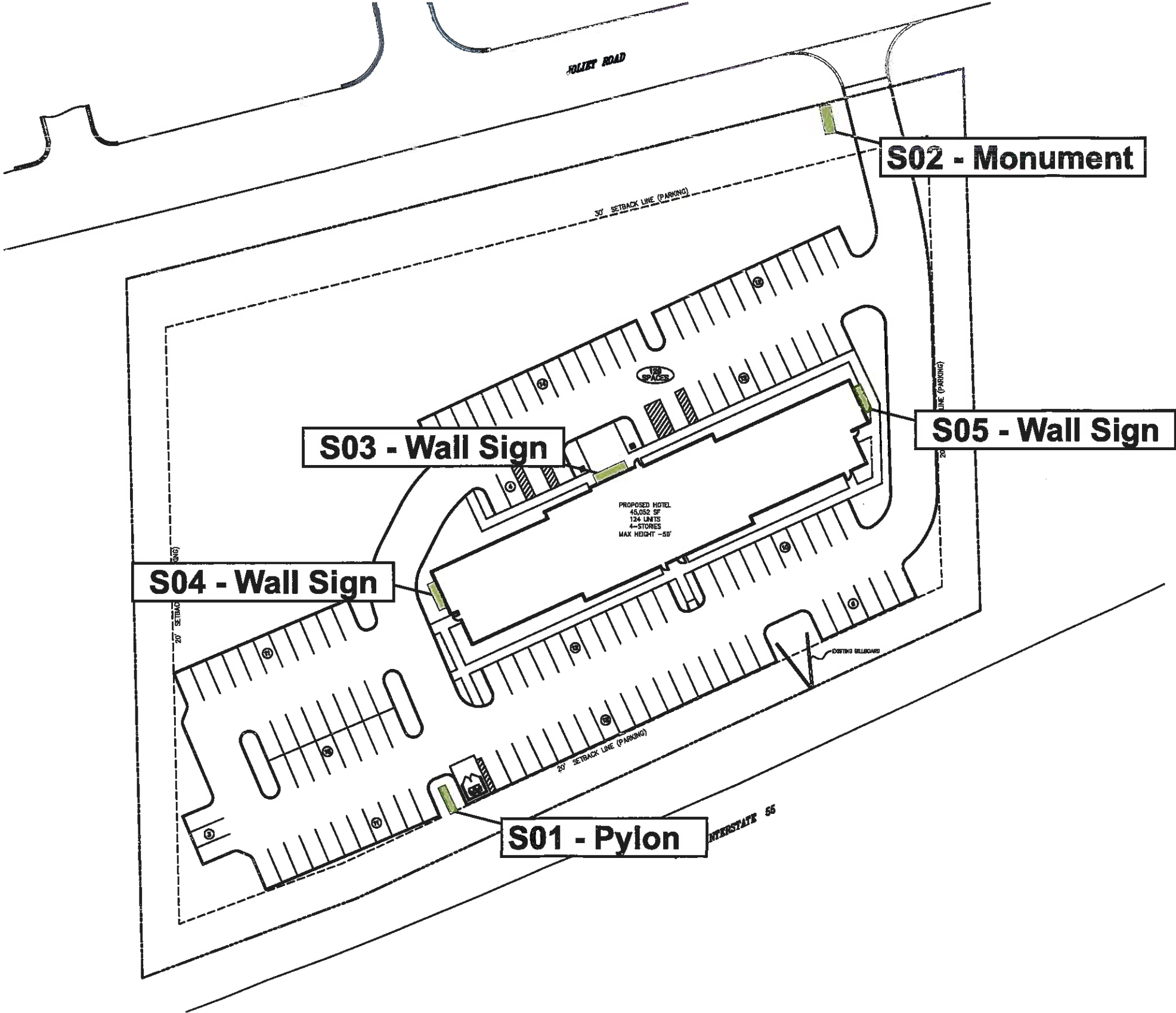


B TRASH ENCLOSURE GATE ELEVATION
1/2" = 1'-0"



A TRASH ENCLOSURE PLAN
1/2" = 1'-0"

SITE MAP



LOCATION #: _____
 ADDRESS: 951 Frontage Rd
 Darien, IL
 DRAWN BY: JER

DATE: 05/23/15

 PAGE: 2

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2655 International Parkway
 Virginia Beach, VA 23452

RECOMMENDATION: SIGN 01



SIGN DETAILS:

Sign Type: P-125S

**Description: 133.9 sq. ft.
Woodspring Suites pylon**

Sign Text: Woodspring Suites Signature

Restoration SOW: N/A

Electrical SOW: N/A

Landscaping SOW: N/A

Comments: N/A



LOCATION #: _____
 ADDRESS: 951 Frontage Rd
 Darien, IL
 DRAWN BY: JER

DATE: 05/23/15
 SCALE: 1/2" = 1'-0"
 PAGE: 2

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2655 International Parkway
 Virginia Beach, VA 23452

RECOMMENDATION: SIGN 02



SIGN DETAILS:

Sign Type: M-60S

Description: 60.0 sq. ft.

Woodspring Suites monument

Sign Text: Woodspring Suites Signature

Restoration SOW: N/A

Electrical SOW: N/A

Landscaping SOW: N/A

Comments: N/A



LOCATION #:

ADDRESS: 951 Frontage Rd
Darien, IL

DRAWN BY: JER

DATE: 05/23/15

SCALE: 1/2" = 1'-0"

PAGE: 3

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2655 International Parkway
Virginia Beach, VA 23452

RECOMMENDATION: SIGN 03



SIGN DETAILS:

Sign Type: W-125-FF

**Description: 128.26 sq. ft.
Woodspring Suites Flex Face Wall Sign**

Sign Text: Woodspring Suites Signature

Restoration SOW: N/A

Electrical SOW: N/A

Landscaping SOW: N/A

Comments: N/A



LOCATION #:

ADDRESS: 951 Frontage Rd
Darlen, IL

DRAWN BY: JER

DATE: 05/23/15

SCALE: 1/8" = 1'-0"

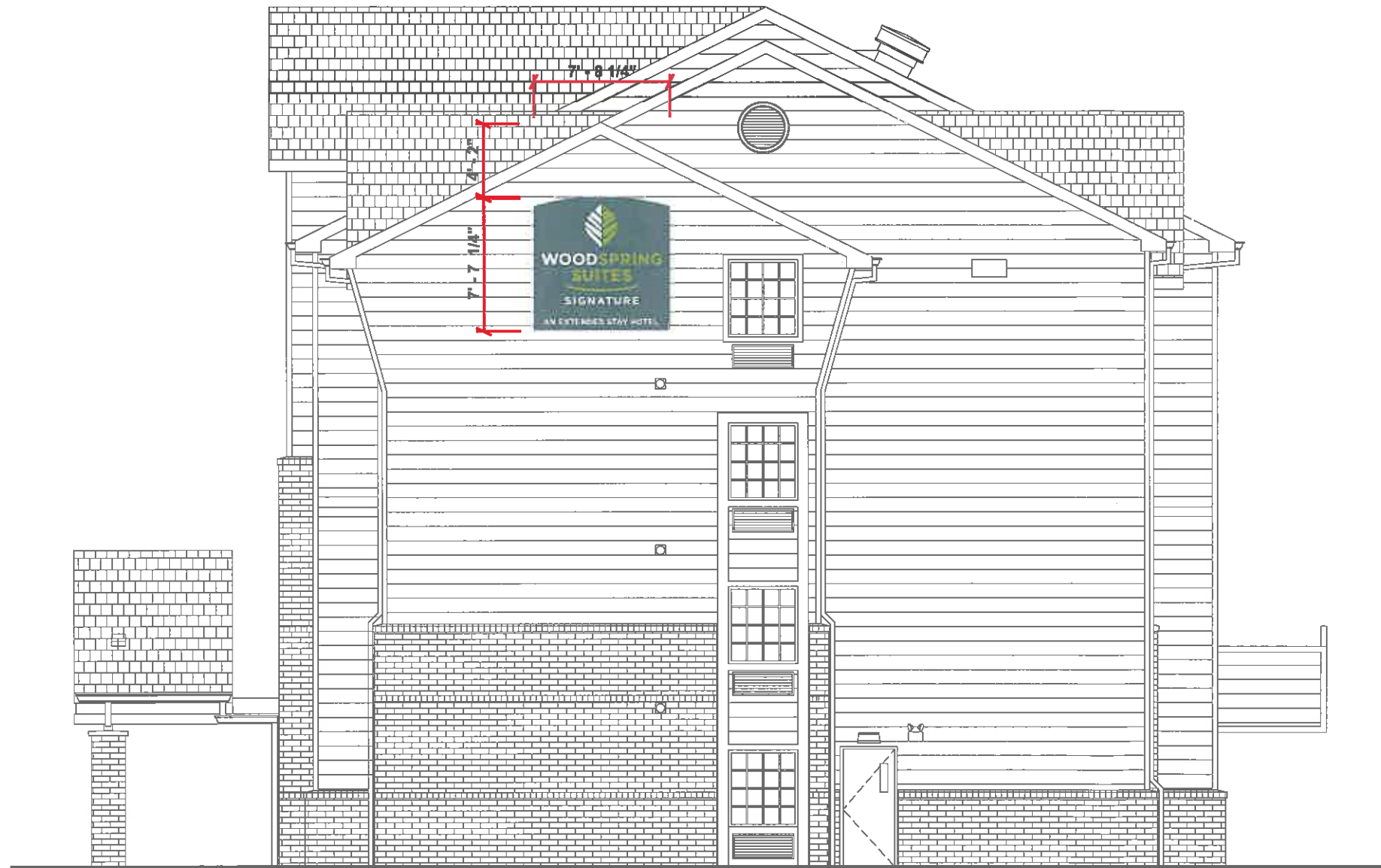
PAGE: 4

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2655 International Parkway
Virginia Beach, VA 23452

RECOMMENDATION: SIGN 04



SIGN DETAILS:

Sign Type: W-60-FF

**Description: 58.46 sq. ft.
Woodspring Suites Flex Face Wall Sign**

Sign Text: Woodspring Suites Signature

Restoration SOW: N/A

Electrical SOW: N/A

Landscaping SOW: N/A

Comments: N/A



LOCATION #:

ADDRESS: 951 Frontage Rd

Darien, IL

DRAWN BY: JER

DATE: 05/23/15

SCALE: 1/8" = 1'-0"

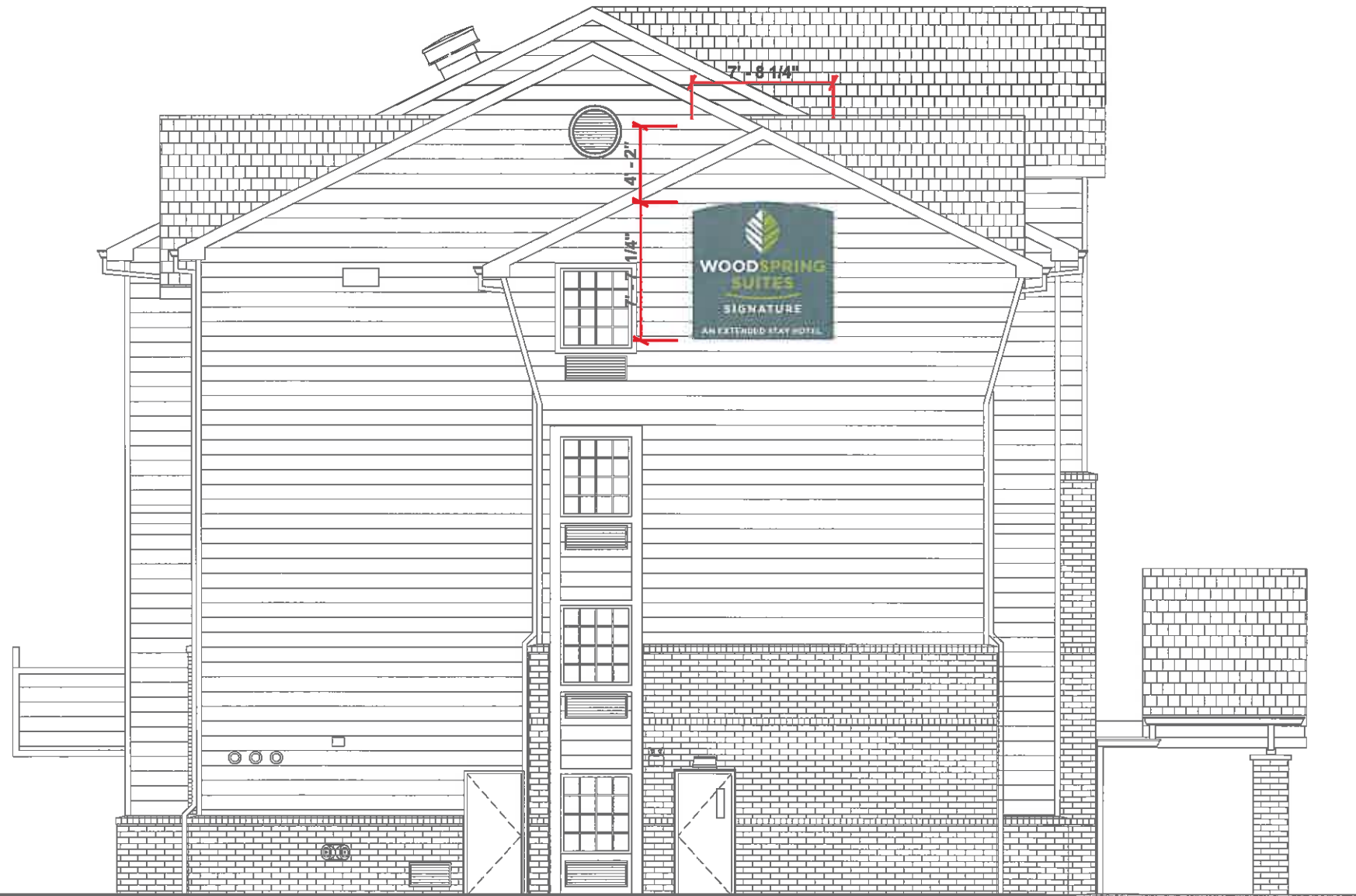
PAGE: 5

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2655 International Parkway
Virginia Beach, VA 23452

RECOMMENDATION: SIGN 05



SIGN DETAILS:

Sign Type: W-60-FF

**Description: 58.46 sq. ft.
Woodspring Suites Flex Face Wall Sign**

Sign Text: Woodspring Suites Signature

Restoration SOW: N/A

Electrical SOW: N/A

Landscaping SOW: N/A

Comments: N/A



LOCATION #:
ADDRESS: 951 Frontage Rd
Darien, IL
DRAWN BY: JER

DATE: 05/23/15
SCALE: 1/8" = 1'-0"
PAGE: 6

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2655 International Parkway
Virginia Beach, VA 23452

AGENDA MEMO
Municipal Services Committee
July 27, 2015

ISSUE STATEMENT

A **resolution** accepting the unit price proposal from Beary Landscaping for the purchase and installation of the 50/50 Parkway Tree Program, Parkway Tree Program, 75th Street Tree Planting, and Wilton Court Special Planting in an amount not to exceed:

Option A: in the amount of \$124,952.50

OR

Option B: in the amount of \$148,769.50

BACKGROUND/HISTORY

The 2015 Tree Planting Program calls out for the following:

- **City and Resident 50/50 Parkway Tree Planting Program**
Staff has included \$4,000 (\$2,000 City funding and \$2,000 Resident funding) cost for residents that would like to participate in the 50/50 program.
- **Storm, Ash Tree, and Project Related Replacement**
The program would allow for the planting of 2.5-inch caliper trees as part of the replacement of trees removed due to the Emerald Ash Borer.
- **Rolling Knolls Wilton Residents Special (Pending City Council Approval of the Final Plat)**
The developer will be required to deposit funds in the amount of \$12,500 and as per the landscape plan on file.
- **75th Street Replacement**
The program would allow for the planting of 4-inch caliper trees as part of the replacement of trees removed from 75th Street, outside of the existing construction limits.

Staff had advertised and sent out nine invitations to bid and received one sealed bid on July 20, 2015, (see **Attachment 1**-Bid Tab) from Beary Landscaping. Staff had contacted various vendors inquiring as to why a bid was not submitted. The feedback included the following:

- Too Busy
- We bid in the past and consistently get beat out by the competition
- Corporate never forwarded the bid to the local office
- Not in a position to commit to the availability of nursery trees

The program includes various selections of 2 ½ - 4 inch caliper trees, planting, mulching and a one-year guarantee. Beary Landscaping completed the work for the City for the 2014 program with satisfactory results. The program again includes a Private Property Tree Planting Program for residents at a pass through cost, see **Attachment 2**. The program includes various selections of 2 ½ - caliper trees, planting, mulching and a one-year guarantee. While the City is administering the

Private Property Tree Planting Program, the property owner would be responsible for ordering and providing a payment directly to the awarded vendor.

This year's Tree Planting Program has seen an average of a 30% price increase due to the great demand for trees. The program is targeted to begin in late September, pending weather and will be advertised through the City's various media portals thus affording residents ample time to participate in the program.

The majority of trees being replaced this year are due to the removals of Ash Trees that were infected by the Emerald Ash tree borer. This year was also the second round of the Ash Tree treatment program and attached is an updated inventory labeled as **Attachment 3**. While the Ash Tree Treatment Program realized a savings of approximately \$25,900, the remaining funds would be utilized for Ash Tree removals that were not able to be saved and the request for expenditure is covered under a separate agenda memo.

Due to the unexpected tree pricing increase the Committee is requested to review two expenditure options labeled as **Option A** and **Option B**. In summary, Staff has determined that the tree program would be approximately \$25,269 over budget should Option B be recommended. The shortfall could be covered by the savings realized from the 2015 Road Program.

STAFF RECOMMENDATION

Staff recommends approval of Option B, accepting the unit price proposal from Beary Landscaping for the purchase and installation of the 50/50 Parkway Tree Program and the planting of various parkway trees in an amount not to exceed \$148,769.50.

ALTERNATE CONSIDERATION

Option A: in the amount of \$124,952.50

DECISION MODE

This item will be placed on the August 3, 2015 City Council agenda for formal consideration.

2015- CITY OF DARIEN TREE BID - PARKWAYS

ATTACHMENT 1

BID OPENING RESULTS JULY 20, 2015

VENDOR			Beary Landscaping
VARIABLE QUANTITY	DIAMETER	SPECIES	PRICE PER TREE PLANTED PARKWAYS
1-10	4"	American Linden	\$ 600.00
11-20	4"	American Linden	\$ 600.00
21-30	4"	American Linden	\$ 600.00
1-10	4"	Norway Maple	\$ 600.00
11-20	4"	Norway Maple	\$ 600.00
21-30	4"	Norway Maple	\$ 600.00
1 – 10	4"	Thomless Honeylocust	\$ 600.00
11-20	4"	Thomless Honeylocust	\$ 600.00
21-30	4"	Thomless Honeylocust	\$ 600.00
1 – 10	4"	Red Maple	\$ 705.00
11-20	4"	Red Maple	\$ 705.00
21-30	4"	Red Maple	\$ 705.00
1 – 10	4"	Chanticleer Pear	\$ 627.00
11-20	4"	Chanticleer Pear	\$ 627.00
21-30	4'	Chanticleer Pear	\$ 627.00
1 – 10	4'	Freeman Maple	\$ 650.00
11-20	4'	Freeman Maple	\$ 650.00
21-30	4'	Freeman Maple	\$ 650.00
1 – 10	4'	Sugar Maple	\$ 645.00
11-20	4'	Sugar Maple	\$ 645.00
21-30	4'	Sugar Maple	\$ 645.00
1 – 10	2.5"	American Linden	\$ 375.00
11-20	2.5"	American Linden	\$ 375.00
21-30	2.5"	American Linden	\$ 375.00
1 – 10	2.5"	Norway Maple	\$ 375.00
11-20	2.5"	Norway Maple	\$ 375.00
21-30	2.5"	Norway Maple	\$ 375.00
1 – 10	2.5"	Honeylocust Skyline	\$ 370.00
11-20	2.5"	Honeylocust Skyline	\$ 370.00
21-30	2.5"	Honeylocust Skyline	\$ 370.00
1-10	2.5"	Red Maple	\$ 440.00
11-20	2.5"	Red Maple	\$ 440.00
21-30	2.5"	Red Maple	\$ 440.00
1-10	2.5"	Chanticleer Pear	\$ 385.00
11-20	2.5"	Chanticleer Pear	\$ 385.00
21-30	2.5"	Chanticleer Pear	\$ 385.00
1-10	2.5"	Freeman Maple	\$ 420.00
11-20	2.5"	Freeman Maple	\$ 420.00
21-30	2.5"	Freeman Maple	\$ 420.00
1-10	2.5"	Sugar Maple	\$ 400.00
11-20	2.5"	Sugar Maple	\$ 400.00
1-30	2.5"	Sugar Maple	\$ 400.00
1-15	6-foot high	Colorado Blue Spruce	\$ 300.00

2015- CITY OF DARIEN TREE BID - PRIVATE PROERTY**ATTACHMENT 2****BID OPENING RESULTS JULY 20, 2015**

VENDOR			Beary Landscaping
VARIABLE QUANTITY	DIAMETER	SPECIES	PRICE PER TREE PLANTED PRIVATE
1 – 20	2.5"	American Linden	\$ 435.00
1 – 20	2.5"	Norway Maple	\$ 435.00
1 – 20	2.5"	Honeylocust Skyline	\$ 435.00
1 – 20	2.5"	Red Maple	\$ 475.00
1 – 20	2.5"	Chanticleer Pear	\$ 440.00
1 – 20	2.5"	Aristocrat Pear	\$ 440.00
1 – 20	2.5"	Swamp White Oak	\$ 460.00
1 – 15	2.5"	Freeman Maple	\$ 460.00
1 – 15	2.5"	Sugar Maple	\$ 445.00
1 – 15	2.5"	Kentucky Coffeetree	\$ 465.00
1-10	2.5"	Purple Maple	\$ 475.00
1-10	2.5"	Northern Red Oak	\$ 460.00
1-10	2.5"	Littleleaf Linden	\$ 435.00
1-10	2.5"	Smoothleaf Elm	\$ 440.00

ITEM	A	B	C	D	E	F	G	H	I	J	K	L	M
1	CITY OF DARIEN TREE PLANTING PROGRAM SCHEDULE 2015 OPTION B												
2	CITY TREE REPLACEMENTS-ACCIDENT TREE REPLACEMENTS-50/50 TREE PROGRAM												
3	VENDOR-BEARLY LANDSCAPING												
4	50/50 TREE PROGRAM												
5	50/50 PARTICIPATION TRD	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	50/50 PROGRAM	CITY COSTS	CONFIRMED CITY PARTICIPATION	CONFIRMED CITY COST	RESIDENT COST			
6	0	2.5"	American Linden	\$375.00	\$ 375.00	1	\$ 187.50	0	\$ -	\$ 187.50			
7	0	2.5"	Norway Maple	\$375.00	\$ 750.00	2	\$ 375.00	0	\$ -	\$ 750.00			
8	0	2.5"	Honeylocust Skyline	\$370.00	\$ 740.00	2	\$ 370.00	0	\$ -	\$ 740.00			
9	0	2.5"	Red Maple	\$440.00	\$ 440.00	1	\$ 220.00	0	\$ -	\$ 220.00			
10	0	2.5"	Chamicleer Pear	\$385.00	\$ 770.00	2	\$ 385.00	0	\$ -	\$ 770.00			
11	0	2.5"	Freeman Maple	\$420.00	\$ 420.00	1	\$ 210.00	0	\$ -	\$ 210.00			
12	0	2.5"	Sugar Maple	\$400.00	\$ 400.00	1	\$ 200.00	0	\$ -	\$ 200.00			
13	TOTALS				\$ 3,895.00	10	\$ 1,947.50	-	\$ -	\$ 1,947.50			3,895.00
14	75TH STREET REPLACEMENT 4-INCH												
15	QUANTITY	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	CITY COSTS							
16	1	4"	American Linden	\$600.00	\$ 600.00	\$ 600.00							
17	1	4"	Norway Maple	\$600.00	\$ 600.00	\$ 600.00							
18	1	4"	Honeylocust Skyline	\$600.00	\$ 600.00	\$ 600.00							
19	1	4"	Red Maple	\$705.00	\$ 705.00	\$ 705.00							
20	1	4"	Chamicleer Pear	\$627.00	\$ 627.00	\$ 627.00							
21	0	4"	Freeman Maple	\$650.00	\$ -	\$ -							
22	0	4"	Sugar Maple	\$645.00	\$ -	\$ -							
23	TOTALS	5			\$ 3,132.00	\$ 3,132.00							
23	PARKWAY TREE REPLACEMENTS-2.5-INCH-MANNING ROAD												
24	QUANTITY	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	CITY COSTS							
25	43	2.5"	American Linden	\$375.00	\$ 16,125.00	\$ 16,125.00							
26	43	2.5"	Norway Maple	\$375.00	\$ 16,125.00	\$ 16,125.00							
27	43	2.5"	Honeylocust Skyline	\$370.00	\$ 15,910.00	\$ 15,910.00							
28	53	2.5"	Red Maple	\$440.00	\$ 23,320.00	\$ 23,320.00							
29	43	2.5"	Chamicleer Pear	\$385.00	\$ 16,555.00	\$ 16,555.00							
30	43	2.5"	Freeman Maple	\$420.00	\$ 18,060.00	\$ 18,060.00							
31	53	2.5"	Sugar Maple	\$400.00	\$ 21,200.00	\$ 21,200.00							
32	0	2.5"	Blue Colorado Spruce	\$300.00	\$ -	\$ -							
33	TOTALS	321			\$ 127,295.00	\$ 127,295.00							
34				TOTAL TREE COST		PARKWAY TREE REPLACEMENT COSTS	CITY COST 50/50 PROGRAM	RESIDENT COST 50/50 PROGRAM	WILTON RESIDENTS SPECIAL	MATH CHECK			
35	TOTAL TREE PROGRAM COST				\$ 134,322.00	\$ 130,427.00	\$ 3,895.00	\$ 1,947.50	\$ 12,500.00	\$ 148,769.50			

ACCOUNT NUMBER	ACCOUNT DESCRIPTION	FY 15/16 BUDGET	FUNDS EXPENDED TO DATE	PROPOSED EXPENDITURE	REINBURSEMENT	PROPOSED BALANCE
01-30-4350	FORESTRY-RESIDENTIAL 50/50 PROGRAM - AND PARKWAY REPLACEMENTS City Expense	\$4,000.00	\$0.00	\$ 3,895.00	City Expense	\$105.00
01-30-4350	FORESTRY-RESIDENTIAL 50/50 PROGRAM - Resident Reimbursement Expense	\$2,000.00	\$0.00	\$ 1,947.50	\$ 200.00	\$52.50
01-30-4350	Forestry - Parkway Replacement Trees-75Th Street	\$2,000.00	\$0.00	\$ 3,132.00	City Expense	(\$1,132.00)
01-30-4350	Forestry - Parkway Replacement Trees-City Removals	\$105,000.00	\$0.00	\$ 127,295.00	City Expense	(\$22,295.00)
	Forestry - WILTON COURT-SPECIAL	\$0.00	\$0.00	\$ 12,500.00	\$ 12,500.00	-
	CONTINGENCY	\$0.00	\$0.00	\$ 2,000.00	\$ -	(\$2,000.00)
TOTALS		\$113,000.00	\$0.00	\$148,769.50	\$12,700.00	(\$25,269.50)

ITEM	A	B	C	D	E	F	G	H	I	J	K
1	CITY OF DARIEN TREE PLANTING PROGRAM SCHEDULE 2015 OPTION A										
2	CITY TREE REPLACEMENTS-ACCIDENT TREE REPLACEMENTS-50/50 TREE PROGRAM										
3	VENDOR-BEARLY LANDSCAPING										
4	50/50 TREE PROGRAM										
5	50/50 PARTICIPATION TBD	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	50/50 PROGRAM	CITY COSTS	CONFIRMED CITY PARTICIPATION	CONFIRMED CITY COST	RESIDENT COST	
6	0	2.5"	American Linden	\$375.00	\$ 375.00	1	\$ 187.50	0	\$ -	\$ 187.50	
7	0	2.5"	Norway Maple	\$375.00	\$ 750.00	2	\$ 375.00	0	\$ -	\$ 750.00	
8	0	2.5"	Honeylocust Skyline	\$370.00	\$ 740.00	2	\$ 370.00	0	\$ -	\$ 740.00	
9	0	2.5"	Red Maple	\$440.00	\$ 440.00	1	\$ 220.00	0	\$ -	\$ 220.00	
10	0	2.5"	Chanticleer Pear	\$385.00	\$ 770.00	2	\$ 385.00	0	\$ -	\$ 770.00	
11	0	2.5"	Freeman Maple	\$420.00	\$ 420.00	1	\$ 210.00	0	\$ -	\$ 210.00	
12	0	2.5"	Sugar Maple	\$400.00	\$ 400.00	1	\$ 200.00	0	\$ -	\$ 200.00	
13	TOTALS			\$ 3,895.00	\$ 10	\$ 1,947.50	\$ -	\$ -	\$ 1,947.50	\$ 3,895.00	
14	75TH STREET REPLACEMENT 4-INCH										
15	QUANTITY	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	CITY COSTS					
16	1	4"	American Linden	\$600.00	\$ 600.00	\$ 600.00					
17	1	4"	Norway Maple	\$600.00	\$ 600.00	\$ 600.00					
18	1	4"	Honeylocust Skyline	\$600.00	\$ 600.00	\$ 600.00					
19	0	4"	Red Maple	\$705.00	\$ -	\$ -					
20	0	4"	Chanticleer Pear	\$627.00	\$ -	\$ -					
21	0	4"	Freeman Maple	\$650.00	\$ -	\$ -					
22	0	4"	Sugar Maple	\$645.00	\$ -	\$ -					
23	TOTALS	3		\$ 1,800.00	\$ 1,800.00	\$ 1,800.00					
23	PARKWAY TREE REPLACEMENTS-2.5-INCH-MANNING ROAD										
24	QUANTITY	DIAMETER	SPECIES	PLANTED PRICE PER TREE	TOTAL TREE COST	CITY COSTS					
25	34	2.5"	American Linden	\$375.00	\$ 12,750.00	\$ 12,750.00					
26	35	2.5"	Norway Maple	\$375.00	\$ 13,125.00	\$ 13,125.00					
27	35	2.5"	Honeylocust Skyline	\$370.00	\$ 12,950.00	\$ 12,950.00					
28	45	2.5"	Red Maple	\$440.00	\$ 19,800.00	\$ 19,800.00					
29	35	2.5"	Chanticleer Pear	\$385.00	\$ 13,475.00	\$ 13,475.00					
30	35	2.5"	Freeman Maple	\$420.00	\$ 14,700.00	\$ 14,700.00					
31	45	2.5"	Sugar Maple	\$400.00	\$ 18,000.00	\$ 18,000.00					
32	0	2.5"	Blue Colorado Spruce	\$300.00	\$ -	\$ -					
33	TOTALS	264		\$ 104,800.00	\$ 104,800.00	\$ 104,800.00					
34					TOTAL TREE COST	PARKWAY TREE REPLACEMENT COSTS	CITY COST 50/50 PROGRAM	RESIDENT COST 50/50 PROGRAM	WILTON RESIDENTS SPECIAL	MATH CHECK	
35	TOTAL TREE PROGRAM COST				\$ 110,495.00	\$ 106,600.00	\$ 3,895.00	\$ 1,947.50	\$ 12,500.00	\$ 124,942.50	

ACCOUNT NUMBER	ACCOUNT DESCRIPTION	FY 15/16 BUDGET	FUNDS EXPENDED TO DATE	PROPOSED EXPENDITURE	REIMBURSEMENT	PROPOSED BALANCE
01-30-4350	FORESTRY-RESIDENTIAL 50/50 PROGRAM - AND PARKWAY REPLACEMENTS City Expense	\$4,000.00	\$0.00	\$ 3,895.00		\$105.00
01-30-4350	FORESTRY-RESIDENTIAL 50/50 PROGRAM - Resident Reimbursement Expense	\$2,000.00	\$0.00	\$ 1,947.50	\$ 1,947.50	\$52.50
01-30-4350	Forestry - Parkway Replacement Trees-75th Street	\$2,000.00	\$0.00	\$ 1,800.00	\$ -	\$200.00
01-30-4350	Forestry - Parkway Replacement Trees-City Removals	\$105,000.00	\$0.00	\$ 104,800.00	\$ -	\$200.00
	Forestry - WILTON COURT-SPECIAL	\$0.00	\$0.00	\$ 12,500.00	\$ 12,500.00	\$ -
TOTALS		\$113,000.00	\$0.00	\$124,942.50	\$14,447.50	\$557.50

ASH TREES INJECTIONS AND REMOVALS COST SUMMARY

ATTACHMENT 3

DESCRIPTION	ACCOUNT NO	PROJECT CODE	QUANTITY	BUDGET	ACTUAL/PROPOSED COST	COST SAVINGS
*TREE TRIMMING PROGRAM	001-30-4375	SECTION 3	PER UNIT	\$ 54,250.00	\$ 54,250.00	
**EAB INJECTIONS CYCLE 2	25-35-4390	FYE165100	1880	\$ 165,000.00	\$ 139,095.67	\$ 25,904.33 **
TREE REMOVALS	001-30-4375	N/A	SEE INVENTORY NOTE	\$ 27,750.00	SEE BELOW	
STUMP REMOVAL	001-30-4375	N/A	SEE INVENTORY NOTE	\$ 9,600.00	SEE BELOW	
CONTINGENCY EMERALD ASH BORER	001-30-4375	N/A	LUMP SUM	\$ 20,000.00	SEE BELOW	
EMERGENCY-STORM AND HAZARDS	001-30-4375	N/A	LUMP SUM	\$ 15,000.00	SEE BELOW	
CONSULTING PROFESSIONAL SERVICES	03-60-4376	N/A	LUMP SUM	\$ 15,000.00	SEE BELOW	
REMOVALS ASH AND NON ASH SPECIES				N/A		
LIST #1 & MISC	N/A			44	N/A	\$ 16,538.50
LIST #2 BILLING IN PROGRESS	N/A			84	N/A	\$ 25,511.50
LIST #3 BILLING IN PROGRESS	N/A			184	N/A	\$ 57,928.14
LIST #4 TO BE COMPLETED AND MISC TRIMMING	N/A				NA	\$ 5,000.00
RESTORATION AND SUPPLIES	N/A	FYE1640024	LUMP SUM		N/A	\$ 6,593.00
* RESTORATION AND SUPPLIES & MISC TRIMMING	N/A	FYE1640024	LUMP SUM		N/A	\$ 1,650.00
TOTALS				\$ 306,600.00	\$ 306,566.81	\$ 33.19
INVENTORY						
ASH TREE INVENTORY 05/01/15	2233					
ASH TREES INJECTED	1880	41 ASH TREES TREATED BY WATERFALL GLENN-NOT INCLUDED IN INVENTORY				
ASH AND NON ASH TREES REMOVED	312					
PERCENTAGE REMOVED	15.81%					
* INDICATES PROPOSED EXPENDITURE						
** MOTION REQUESTING BALANCE OF \$25,904 TO REMOVE DEAD ASH TREES						

RESOLUTION NO. _____

A RESOLUTION ACCEPTING THE UNIT PRICE PROPOSAL FROM BEARY LANDSCAPING FOR THE PURCHASE AND INSTALLATION OF THE 50/50 PARKWAY TREE PROGRAM, PARKWAY TREE PROGRAM, 75TH STREET TREE PLANTING, AND WILTON COURT SPECIAL PLANTING IN THE AMOUNT NOT TO EXCEED \$ _____

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DARIEN, DU PAGE COUNTY, ILLINOIS, as follows:

SECTION 1: The City Council of the City of Darien does hereby approves a Resolution Accepting the Unit Price Proposal from Beary Landscaping for the Purchase and Installation of the 50/50 Parkway Tree Program, Parkway Tree Program, 75th Street Tree Planting, and Wilton Court Special Planting in the Amount not to Exceed \$ _____, a copy of which is attached hereto as “**Exhibit A**” and is by this reference expressly incorporated herein.

SECTION 2: This Resolution shall be in full force and effect from and after its passage and approval as provided by law.

PASSED BY THE CITY COUNCIL OF THE CITY OF DARIEN, DU PAGE COUNTY, ILLINOIS, this 3rd day of August, 2015.

AYES: _____

NAYS: _____

ABSENT: _____

APPROVED BY THE MAYOR OF THE CITY OF DARIEN, DU PAGE COUNTY, ILLINOIS, this 3rd day of August, 2015.

KATHLEEN MOESLE WEAVER, MAYOR

ATTEST:

JOANNE E. RAGONA, CITY CLERK

APPROVED AS TO FORM:

CITY ATTORNEY

CITY OF DARIEN

CONTRACT

This Contract is made this ____ day of _____, 20__ by and between the City of Darien (hereinafter referred to as the "CITY") and _____ (hereinafter referred to as the "CONTRACTOR").

WITNESSETH

In consideration of the promises and covenants made herein by the CITY and the CONTRACTOR (hereinafter referred to collectively as the "PARTIES"), the PARTIES agree as follows:

SECTION 1: THE CONTRACT DOCUMENTS: This Contract shall include the following documents (hereinafter referred to as the "CONTRACT DOCUMENTS") however this Contract takes precedence and controls over any contrary provision in any of the CONTRACT DOCUMENTS. The Contract, including the CONTRACT DOCUMENTS, expresses the entire agreement between the PARTIES and where it modifies, adds to or deletes provisions in other CONTRACT DOCUMENTS; the Contract's provisions shall prevail. Provisions in the CONTRACT DOCUMENTS unmodified by this Contract shall be in full force and effect in their unaltered condition.

The Invitation to Bid

The Instructions to the Bidders

This Contract

The Terms and Conditions

The Bid as it is responsive to the CITY'S bid requirements

All Certifications required by the City

Certificates of insurance

Performance and Payment Bonds as may be required by the CITY

SECTION 2: SCOPE OF THE WORK AND PAYMENT: The CONTRACTOR agrees to provide labor, equipment and materials necessary to provide the services as described in the CONTRACT DOCUMENTS and further described below:

Tree Planting and Mulching for parkways within the City of Darien and Tree Planting and Mulching for private properties-Unit Pricing

(Hereinafter referred to as the "WORK") and the CITY agrees to pay the CONTRACTOR pursuant to the provisions of the Local Government Prompt Payment Act (50 ILCS 505/1 *et seq.*) the following amount for performance of the described unit prices.

SECTION 3: ASSIGNMENT: CONTRACTOR shall not assign the duties and obligations involved in the performance of the WORK which is the subject matter of this Contract without the written consent of the CITY.

SECTION 4: TERM OF THE CONTRACT: This Contract shall commence on the date of its execution. The WORK shall commence upon receipt of a Notice to Proceed and continue expeditiously for 30 days from that date until final completion. This Contract shall terminate upon completion of the WORK, but may be terminated by either of the PARTIES for default upon failure to cure after ten (10) days prior written notice of said default from the aggrieved PARTY. The CITY, for its convenience, may terminate this Contract with thirty (30) days prior written notice.

SECTION 5: INDEMNIFICATION AND INSURANCE: The CONTRACTOR shall indemnify and hold harmless the CITY, PARK DISTRICT, its officials, officers, directors, agents, employees and representatives and assigns, from lawsuits, actions, costs (including attorneys' fees), claims or liability of any character, incurred due to the alleged negligence of the CONTRACTOR, brought because of any injuries or damages received or sustained by any person, persons or property on account of any act or omission, neglect or misconduct of said

CONTRACTOR, its officers, agents and/or employees arising out of, or in performance of any of the provisions of the CONTRACT DOCUMENTS, including any claims or amounts recovered for any infringements of patent, trademark or copyright; or from any claims or amounts arising or recovered under the "Worker's Compensation Act" or any other law, ordinance, order or decree. In connection with any such claims, lawsuits, actions or liabilities, the CITY, its officials, officers, directors, agents, employees, representatives and their assigns shall have the right to defense counsel of their choice. The CONTRACTOR shall be solely liable for all costs of such defense and for all expenses, fees, judgments, settlements and all other costs arising out of such claims, lawsuits, actions or liabilities. The Contractor shall not make any settlement or compromise of a lawsuit or claim, or fail to pursue any available avenue of appeal of any adverse judgment, without the approval of the City and any other indemnified party. The City or any other indemnified party, in its or their sole discretion, shall have the option of being represented by its or their own counsel. If this option is exercised, then the Contractor shall promptly reimburse the City or other indemnified party, upon written demand, for any expenses, including but not limited to court costs, reasonable attorneys' and witnesses' fees and other expenses of litigation incurred by the City or other indemnified party in connection therewith. Execution of this Contract by the CITY is contingent upon receipt of Insurance Certificates provided by the CONTRACTOR in compliance with the CONTRACT DOCUMENTS.

SECTION 6: COMPLIANCE WITH LAWS: The bidder shall at all times observe and comply with all laws, ordinances and regulations of the federal, state, local and City governments, which may in any manner affect the preparation of bids or the performance of the Contract. Bidder hereby agrees that it will comply with all requirements of the Illinois Human Rights Act, 775 ILCS 5/1-101 et seq., including the provision dealing with sexual harassment and that if awarded the Contract will not engage in any prohibited form of discrimination in employment as defined in that Act and will require that its subcontractors agree to the same restrictions. The contractor shall maintain, and require that its subcontractors maintain, policies of equal employment opportunity which shall prohibit discrimination against any employee or applicant for employment on the basis of race, religion, color, sex, national origin, ancestry, citizenship status, age, marital status, physical or mental disability unrelated to the individual's ability to perform the essential functions of the job, association with a person with a disability, or unfavorable discharge from military service. Contractors and all subcontractors shall comply with all requirements of the Act and of the Rules of the Illinois Department of Human Rights with regard to posting information on employees' rights under the Act. Contractors and all subcontractors shall place appropriate statements identifying their companies as equal opportunity employers in all advertisements for workers to be employed in work to be performed. Not less than the Prevailing Rate of Wages as found by the City of Darien or the Department of Labor shall be paid to laborers, workmen, and mechanics performing work under the Contract. If awarded the Contract, contractor must comply with all provisions of the Illinois Prevailing Wage Act, including, but not limited to, providing certified payroll records to the Municipal Services Department. Contractor and subcontractors shall be required to comply with all applicable federal laws, state laws and regulations regarding minimum wages, limit on payment to minors, minimum fair wage standards for minors, payment of wage due employees, and health and safety of employees. Contractor and subcontractor are required to pay employees all rightful salaries, medical benefits, pension and social security benefits pursuant to applicable labor agreements and federal and state statutes and to further require withholdings and deposits therefore. The CONTRACTOR shall obtain all necessary local and state licenses and/or permits that may be required for performance of the WORK and provide those licenses to the CITY prior to commencement of the WORK if applicable.

SECTION 7: NOTICE: Where notice is required by the CONTRACT DOCUMENTS

it shall be considered received if it is delivered in person, sent by registered United States mail, return receipt requested, delivered by messenger or mail service with a signed receipt, sent by facsimile or e-mail with an acknowledgment of receipt, to the following:

City of Darien
1702 Plainfield Road
Darien, IL 60561

Attn: Director of Municipal Services

SECTION 8: STANDARD OF SERVICE: Services shall be rendered to the highest professional standards to meet or exceed those standards met by others providing the same or similar services in the Chicagoland area. Sufficient competent personnel shall be provided who with supervision shall complete the services required within the time allowed for performance. The CONTRACTOR’S personnel shall, at all times present a neat appearance and shall be trained to handle all contact with City residents or City employees in a respectful manner. At the request of the City Administrator or a designee, the CONTRACTOR shall replace any incompetent, abusive or disorderly person in its employ.

SECTION 9: PAYMENTS TO OTHER PARTIES: The CONTRACTOR shall not obligate the CITY to make payments to third parties or make promises or representations to third parties on behalf of the CITY without prior written approval of the City Administrator or a designee.

SECTION 10: COMPLIANCE: CONTRACTOR shall comply with all of the requirements of the Contract Documents, including, but not limited to, the Illinois Prevailing Wage Act where applicable and all other applicable local, state and federal statutes, ordinances, codes, rules and regulations.

SECTION 11: LAW AND VENUE: The laws of the State of Illinois shall govern this Contract and venue for legal disputes shall be DuPage County, Illinois.

SECTION 12: MODIFICATION: This Contract may be modified only by a written amendment signed by both PARTIES.

FOR: THE CITY

FOR: THE CONTRACTOR

By: _____

By: _____

Print Name: _____

Print Name: _____

Title: Mayor

Title: _____

Date: _____

Date: _____

Vendor Information

The project is scheduled for a September 15, 2015 planting, and to be completed by no later than October 31, 2015.

TO BE COMPLETED BY VENDOR-PRINT CLEARLY

COMPANY NAME: BEARY LANDSCAPING, INC.

CONTACT PERSON: MIKE FLYNN

ADDRESS: 15001 W. 159TH STREET

CITY, STATE, ZIP CODE: LOCKPORT. IL 60491

TELEPHONE NUMBER: Office 815-838-4100 Mobile 708-774-3406

FACSIMILE NUMBER: 815-838-3200

E-MAIL ADDRESS m.flynn@bearylandscaping.com

AUTHORIZED SIGNATURE: 

CITY OF DARIEN TREE QUOTE – PARKWAYS

VARIABLE QUANTITY	DIAMETER	SPECIES	PRICE PER TREE PLANTED
1 – 10	4"	American Linden	600
11 – 20	4"	American Linden	600
21 – 30	4"	American Linden	600
1 – 10	4"	Norway Maple	600
11 – 20	4"	Norway Maple	600
21 – 30	4"	Norway Maple	600
1 – 10	4"	Thornless Honeylocust	600
11 – 20	4"	Thornless Honeylocust	600
21 – 30	4"	Thornless Honeylocust	600
1 – 10	4"	Red Maple	705
11 – 20	4"	Red Maple	705
21 – 30	4"	Red Maple	705
1 – 10	4"	Chanticleer Pear	627
11 – 20	4"	Chanticleer Pear	627
21 – 30	4"	Chanticleer Pear	627
1 – 10	4"	Freeman Maple	650
11 – 20	4"	Freeman Maple	650
21 – 30	4"	Freeman Maple	650
1 – 10	4"	Sugar Maple	645
11 – 20	4"	Sugar Maple	645
21 – 30	4"	Sugar Maple	645

1 - 10	2.5"	American Linden	275
11 - 20	2.5"	American Linden	375
21 - 30	2.5"	American Linden	375
1 - 10	2.5"	Norway Maple	375
11 - 20	2.5"	Norway Maple	375
21 - 30	2.5"	Norway Maple	375
1 - 10	2.5"	Honeylocust Skyline	370
11 - 20	2.5"	Honeylocust Skyline	370
21 - 30	2.5"	Honeylocust Skyline	370
1 - 10	2.5"	Red Maple	440
11 - 20	2.5"	Red Maple	440
21 - 30	2.5"	Red Maple	440
1 - 10	2.5"	Chanticleer Pear	385
11 - 20	2.5"	Chanticleer Pear	385
21 - 30	2.5"	Chanticleer Pear	385
1 - 10	2.5"	Freeman Maple	420
11 - 20	2.5"	Freeman Maple	420
21 - 30	2.5"	Freeman Maple	420
1 - 10	2.5"	Sugar Maple	400
11 - 20	2.5"	Sugar Maple	400
1 - 30	2.5"	Sugar Maple	400
1 - 15	6-foot high	Colorado Blue Spruce	300

B. Sub Total Cost Parkway			21,876. ⁰⁰
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CITY OF DARIEN TREE QUOTE - PRIVATE

VARIABLE QUANTITY	DIAMETER	SPECIES	PRICE PER TREE PLANTED
1 - 20	2.5"	American Linden	435
1 - 20	2.5"	Norway Maple	435
1 - 20	2.5"	Honeylocust Skyline	435
1 - 20	2.5"	Red Maple	475
1 - 20	2.5"	Chanticleer Pear	440
1 - 20	2.5"	Aristocrat Pear	440
1 - 20	2.5"	Swamp White Oak	460
1 - 15	2.5"	Freeman Maple	460
1 - 15	2.5"	Sugar Maple	445
1 - 15	2.5"	Kentucky Coffeetree	465
1 - 10	2.5"	Purple Maple	475
1 - 10	2.5"	Northern Red Oak	460
1 - 10	2.5"	Littleleaf Linden	435
1 - 10	2.5"	Smoothleaf Elm	440
B. Sub Total Cost Private Property			6,300. ⁰⁰

**CITY OF DARIEN TREE QUOTE – SPECIAL REAR YARD EASEMENT
LANDSCAPING**

The proposed project incorporates the proposed plantings to be installed as directed by the City within a designated rear yard easement of 5 homes located within the 7600 block of Wilton Court- Landscape plan on file

VARIABLE QUANTITY	DIAMETER	SPECIES	PRICE PER TREE PLANTED
1 – 20	2.5"	Aristocrat Pear	440
1 – 20	2.5"	Swamp White Oak	460
1 – 15	2.5"	Kentucky Coffeetree	405
1 – 10	2.5"	Purple Maple	475
1 – 10	2.5"	Northern Red Oak	460
1 – 10	2.5"	Littleleaf Linden	435
1 – 10	2.5"	Smoothleaf Elm	440
DECIDUOUS SHADE TREES			
1	3"	Acer p Crimson King Norway Maple	580
1	3"	Acer Saccharum Sugar Maple	575
1	3"	Quercus Ellipsoidalis Northern Pin Oak	575
2	3"	Tilla a Redmond Redmond American Linder	490
EVERGREEN TREES			
1	8'	Picea abies Norway Spruce	475
1	8'	Picea g Densata Black Hills Spruce	575
2	10'	Picea g Densata Black Hills Spruce	910
1	8'	Picea Pungens Colorado Green Spruce	475
2	10'	Picea Pungens Colorado Green Spruce	750

DECIDUOUS ORNAMENTAL			
1	2"	Carpinus Caroliniana American Hornbeam	625 ^{EA}
2	6'	Viburnum prunifolium Blackhaw Viburnum	235 ^{EA}
DECIDUOUS SHRUBS			
5	#5	Euonymus a Compactus Dwarf Burning Bush	80 ^{EA}
7	#3	Goldfinger Potentilla	43 ^{EA}
4	3'BB	Viburnum d Ralph Senior Autumn Jazz Arrowwood	60 ^{EA}
MATERIAL & LABOR LIST			
3 CY	(45 bags)	Southern Pine Bark Mulch	175 ^{EA}
0.5 CY		Shredded Hardwood Mulch	95 ^{EA}
ALTERNATE			
Subtract 7	#3	Goldfinger Potentilla	- 43 ^{EA}
Add 1	8'BB	Black Hills Spruce	575 ^{EA}
C. Sub Total Cost Special-Easement			10,425

A. Sub Total Cost Parkways	21,876
B. Sub Total Cost Private Property	6,300
C. Sub Total Cost Special-Easement	10,425
Total Cost of A+B+C	38,601

The quote will be awarded on a total sum of A and B and C.

AGENDA MEMO
Municipal Services Committee
July 27, 2015

ISSUE STATEMENT

A motion authorizing the expenditure of \$25,900 from the Emerald Ash Borer Round Two Treatment Program fund for the Removal of Dead Ash Trees.

BACKGROUND/HISTORY

The City Council authorized phase two of the Emerald Ash Borer Treatment Program on March 15, 2015. The program included treating approximately 2,357 Ash trees with the product Treeage. Attached and labeled as **Attachment 1** is a to date summary of the Treatment and Removal Programs. Prior to the commencement of the treatment program 124 ash trees were removed in April. The ash tree inventory as of May 1, 2015, was revised to 2,233. It was determined during the treatment program that an additional 312 Ash trees could not be saved and required removal.

The budget for the treatment program was \$165,000 and the total program cost was \$139,057 resulting in a net savings of approximately \$25,900. The cost for removals and restorations to date has been estimated to be approximately \$25,900 over budget.

The Staff is requesting a motion authorizing the expenditure of the savings in the amount of \$25,900 to be utilized for the remaining removals under Account No 25-35-4390.

STAFF RECOMMENDATION

Staff recommends approval of a motion authorizing the expenditure of \$25,900 from the Emerald Ash Borer Round Two Treatment Program fund for the Removal of Dead Ash Trees.

ALTERNATE CONSIDERATION

As directed by the Committee

DECISION MODE

This item will be placed on the August 3, 2015 City Council agenda for formal consideration.

ASH TREES INJECTIONS AND REMOVALS COST SUMMARY

ATTACHMENT 1

DESCRIPTION	ACCOUNT NO	PROJECT CODE	QUANTITY	BUDGET	ACTUAL/PROPOSED COST	COST SAVINGS
*TREE TRIMMING PROGRAM	001-30-4375	SECTION 3	PER UNIT	\$ 54,250.00	\$ 54,250.00	
**EAB INJECTIONS CYCLE 2	25-35-4390	FYE165100	1880	\$ 165,000.00	\$ 139,095.67	\$ 25,904.33 **
TREE REMOVALS	001-30-4375	N/A	SEE INVENTORY NOTE	\$ 27,750.00	SEE BELOW	
STUMP REMOVAL	001-30-4375	N/A	SEE INVENTORY NOTE	\$ 9,600.00	SEE BELOW	
CONTINGENCY EMERALD ASH BORER	001-30-4375	N/A	LUMP SUM	\$ 20,000.00	SEE BELOW	
EMERGENCY-STORM AND HAZARDS	001-30-4375	N/A	LUMP SUM	\$ 15,000.00	SEE BELOW	
CONSULTING PROFESSIONAL SERVICES	03-60-4376	N/A	LUMP SUM	\$ 15,000.00	SEE BELOW	
REMOVALS ASH AND NON ASH SPECIES				N/A		
LIST #1 & MISC	N/A			44	N/A	\$ 16,538.50
LIST #2 BILLING IN PROGRESS	N/A			84	N/A	\$ 25,511.50
LIST #3 BILLING IN PROGRESS	N/A			184	N/A	\$ 57,928.14
LIST #4 TO BE COMPLETED AND MISC TRIMMING	N/A				NA	\$ 5,000.00
RESTORATION AND SUPPLIES	N/A	FYE1640024	LUMP SUM		N/A	\$ 6,593.00
* RESTORATION AND SUPPLIES & MISC TRIMMING	N/A	FYE1640024	LUMP SUM		N/A	\$ 1,650.00
TOTALS				\$ 306,600.00	\$ 306,566.81	\$ 33.19
INVENTORY						
ASH TREE INVENTORY 05/01/15	2233					
ASH TREES INJECTED	1880	41 ASH TREES TREATED BY WATERFALL GLENN-NOT INCLUDED IN INVENTORY				
ASH AND NON ASH TREES REMOVED	312					
PERCENTAGE REMOVED	15.81%					
* INDICATES PROPOSED EXPENDITURE						
** MOTION REQUESTING BALANCE OF \$25,904 TO REMOVE DEAD ASH TREES						

AGENDA MEMO
Municipal Services Committee
July 27, 2015

Issue Statement

Approval of a Resolution accepting a Storm Sewer Easement from the following property:

214 Janet Avenue - PIN 09-26-103-031

Background/History

The property owner at 214 Janet Avenue has agreed to grant a storm sewer easement to the City in anticipation of an upcoming rear yard drainage project. The scope of work includes installing an under drain pipe through the proposed side yard easements of the participating property and connect to a storm sewer structure located on Janet Avenue. The easement is required to provide drainage and overland flow swale to rid the area of nuisance ponding and provide positive drainage.

Staff has reached out to the residents for a storm sewer easement to be dedicated only to the City of Darien. The plat requires City Council approval and will be recorded by DuPage County. The following resident has agreed to dedicate a storm sewer easement as per the attached Plat of Easement labeled as Attachment A:

214 Janet Avenue- PIN 09-26-103-031

Staff Recommendation

Staff recommends acceptance of a Storm Sewer Easement from the property listed above.

Alternate Consideration

Not approving the resolution at this time.

Decision Mode

This item will be on the August 3, 2015 City Council agenda for formal consideration.

RESOLUTION NO. _____

**APPROVAL OF A RESOLUTION ACCEPTING A STORM SEWER EASEMENT FROM
THE FOLLOWING PROPERTY:
214 JANET AVENUE – PIN 09-26-103-031**

**BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DARIEN, DU
PAGE COUNTY, ILLINOIS**, as follows:

SECTION 1: The City Council of the City of Darien does hereby accepts a Storm Sewer Easement from the Following Property: 214 Janet Avenue - Pin 09-26-103-031, a copy of which is attached hereto as “**Exhibit A**” and is by this reference expressly incorporated herein.

SECTION 2: This Resolution shall be in full force and effect from and after its passage and approval as provided by law.

**PASSED BY THE CITY COUNCIL OF THE CITY OF DARIEN, DU PAGE
COUNTY, ILLINOIS**, this 3rd day of August, 2015.

AYES: _____

NAYS: _____

ABSENT: _____

**APPROVED BY THE MAYOR OF THE CITY OF DARIEN, DU PAGE
COUNTY, ILLINOIS**, this 3rd day of August, 2015.

KATHLEEN MOESLE WEAVER, MAYOR

ATTEST:

JOANNE E. RAGONA, CITY CLERK

APPROVED AS TO FORM:

CITY ATTORNEY



• BOUNDARY • TOPOGRAPHICAL • SUBDIVISIONS • ALTA/ACSM • CONDOMINIUMS • SITE PLANS • CONSTRUCTION • FEMA CERTIFICATES •

SCHOMIG LAND SURVEYORS, LTD. PLAT OF GRANT OF EASEMENT

909 EAST 31ST STREET
LA GRANGE PARK, ILLINOIS 60526
SCHOMIG-SURVEY@SBCGLOBAL.NET
WWW.LAND-SURVEY-NOW.COM
PHONE: 708-352-1452
FAX: 708-352-1454

LOCATED ON:

LOT 28 IN BLOCK 51 IN TRI STATE VILLAGE UNIT NUMBER 6, BEING A SUBDIVISION OF PART OF THE SOUTHWEST 1/4 OF SECTION 23, AND PART OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DU PAGE COUNTY, ILLINOIS.

OWNERS CERTIFICATE - LOT 28
214 JANET AVENUE

STATE OF ILLINOIS)
COUNTY OF DU PAGE) ss.

WE, RODNEY AVERY AND YOLANDA PASQUINELLI-AVERY DO HEREBY CERTIFY THAT WE ARE THE OWNERS, OF THE PROPERTY DESCRIBED AS:

LOT 28 IN BLOCK 51 IN TRI STATE VILLAGE UNIT NUMBER 6, BEING A SUBDIVISION OF PART OF THE SOUTHWEST 1/4 OF SECTION 23, AND PART OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 38 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DU PAGE COUNTY, ILLINOIS.

AND AS SUCH OWNERS, WE HAVE CAUSED THE SAID PROPERTY TO BE SURVEYED AND THIS PLAT OF GRANT OF EASEMENT TO THE CITY OF DARIEN, FOR DRAINAGE PURPOSES DESCRIBED AS THE SOUTHWESTERLY 10 FEET (EXCEPT THE NORTHWESTERLY 10 FEET THEREOF), OF AFORESAID LOT 28, AS SHOWN HEREON SHOWN TO BE PREPARED AS OUR OWN FREE AND VOLUNTARY ACT AND DEED.

DATED THIS _____ DAY OF _____ A.D. 2015.

OWNER: _____

OWNER: _____

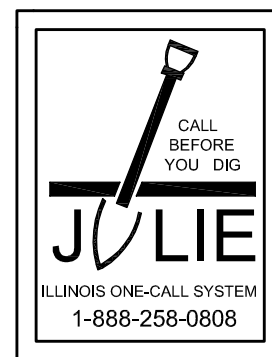
OWNERS NOTARY - LOT 28
214 JANET AVENUE

STATE OF ILLINOIS)
COUNTY OF DU PAGE) ss.

I, _____ A NOTARY PUBLIC IN AND FOR THE COUNTY AND STATE AFORESAID, DO HEREBY CERTIFY THAT RODNEY AVERY AND YOLANDA PASQUINELLI-AVERY ARE PERSONALLY KNOWN BY ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING CERTIFICATE. DID APPEAR BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY ARE THE OWNERS OF THE PROPERTY DESCRIBED AND THAT AS SUCH OWNERS, HAVE CAUSED SAID PROPERTY TO BE SURVEYED AND THIS PLAT OF GRANT OF EASEMENT TO BE PREPARED AT HIS OWN FREE AND VOLUNTARY ACT AND DEED FOR THE PURPOSES AFORESAID.

DATED THIS _____ DAY OF _____ A.D. 2015.

BY: _____ NOTARY PUBLIC.



- LEGEND
- ☉ = CENTER LINE
 - R. = RECORDED DIMENSION
 - ▨ = ASPHALT
 - = TREE
 - W.F. = WOOD FENCE



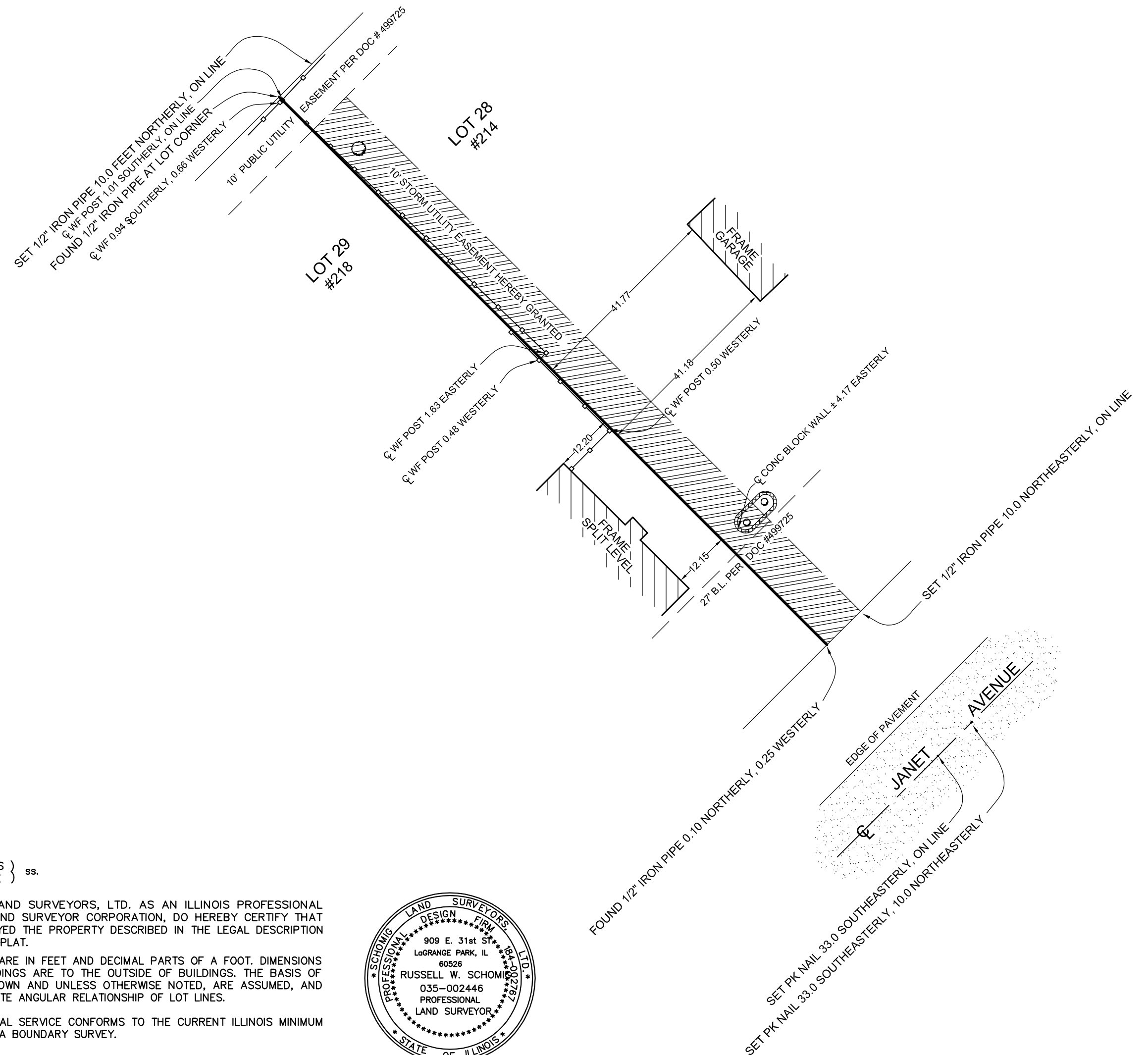
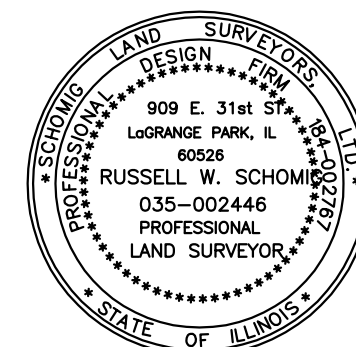
STATE OF ILLINOIS)
COUNTY OF COOK) ss.

WE, SCHOMIG LAND SURVEYORS, LTD. AS AN ILLINOIS PROFESSIONAL DESIGN FIRM, LAND SURVEYOR CORPORATION, DO HEREBY CERTIFY THAT WE HAVE SURVEYED THE PROPERTY DESCRIBED IN THE LEGAL DESCRIPTION AFFIXED TO THIS PLAT.

ALL DIMENSIONS ARE IN FEET AND DECIMAL PARTS OF A FOOT. DIMENSIONS SHOWN ON BUILDINGS ARE TO THE OUTSIDE OF BUILDINGS. THE BASIS OF BEARINGS, IF SHOWN AND UNLESS OTHERWISE NOTED, ARE ASSUMED, AND SHOWN TO INDICATE ANGULAR RELATIONSHIP OF LOT LINES.

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

BY: _____
PROFESSIONAL ILLINOIS LAND SURVEYOR LICENSE # 035-002446



SURVEYORS NOTE: ALL IMPROVEMENTS ARE NOT SHOWN HEREON.

SURVEY DATE: JULY 14TH, 2015.

PLAT NUMBER: 142146 & H22-58

SCALE: 1" = 20'

**CITY OF DARIEN
MUNICIPAL SERVICES COMMITTEE
May 26, 2015**

PRESENT: Joseph Marchese - Chairperson, Alderman Tom Belczak, Dan Gombac – Director, Michael Griffith - Senior Planner, Elizabeth Lahey - Secretary

ABSENT: None

OTHERS: Mayor Kathy Weaver, Michael Coren

ESTABLISH QUORUM

Chairperson Marchese called the meeting of the Municipal Services Committee to order at 6:30 p.m. at the City Hall – City Council Chambers, Darien, Illinois and declared a quorum present.

NEW BUSINESS

- A. Ordinance - Darien Immediate Care, 8190 Cass Avenue - Petitioner seeks approval of a variation from the Sign Code to permit a wall sign on the side of the building which is not the front façade within the O Office Zoning District.**

Mr. Michael Griffith, Senior Planner reported the variation request is for the sign location and not for the size of the sign or illumination. He reported the PZC considered this matter and recommended approval 6-1. He further reported Chairperson Meyer voted against the petition and that there were comments against the petitioner as well.

Chairperson Marchese questioned if there was any other business with a sign on the side.

Mr. Griffith reported that there were no other businesses with a sign on the side. He stated that the request is for an Immediate Care Facility and that patients with emergencies are having a difficult time locating the facility. He reported the PZC felt that because of the nature of the business that there was justification for a sign.

Chairperson Marchese stated he was concerned if others in the office park would be requesting the same type of signage.

Mr. Griffith stated signs are allowed if they face the street. He stated if the property was zoned business they would not need a variation.

Alderman Belczak stated this was a unique situation and they will probably not see such a request again. He stated he cannot foresee why an office would need a variance.

Mr. Michael Coren, 8170 Cass Avenue stated his office building is immediately to the north. He provided some history of the development and stated the area was originally zoned for business but the developer asked the City to zone it office because of the homes in the area. Mr. Coren stated he was concerned for the variance because of aesthetics and that a precedent is being set. He further stated the PZC approved the variance for a certain circumstance and he is

at a significant financial disadvantage should a business want to add a sign. Mr. Coren stated if the City allows one to have a sign they should all be given the same.

Chairperson Marchese stated every situation is looked at individually and the task is to look at ways to help businesses. He stated he likes the look of the office park but he also wants to help out the business.

Alderman Belczak stated it is more to help those with an emergency and not so much the business.

Mr. Coren stated there are other urgent care facilities down the street and the petitioner should have done research beforehand.

Mr. Griffith reported that the Association does not object to a sign on Cass Avenue.

Mr. Dan Gombac, Director questioned the member of the Association.

Mr. Coren stated that each owner has one sixth vote.

There was no one else wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese to approve an Ordinance - Darien Immediate Care, 8190 Cass Avenue -a variation from the Sign Code to permit a wall sign on the side of the building which is not the front façade within the O Office Zoning District.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

B. 113 68th Street - Petitioner seeks approval of the following variations from the Zoning Ordinance in order to construct a new single-family home on an existing foundation: 1. To reduce the required front yard setback from 35 feet to 30.8 feet, 2. To reduce the required west interior side yard from 10 feet to 9.2 feet, within the R-2 Single Family Residence Zoning District.

Mr. Michael Griffith, Senior Planner reported the petitioner's home was destroyed in a fire and the petitioner is proposing to build a new home on the existing foundation with a front porch. He reported the PZC held a public hearing and voted in favor of the petition. He further reported there were no comments or objections.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese approval of the following variations from the Zoning Ordinance in order to construct a new single-family home on an existing foundation: 1. To reduce the required front yard setback from 35 feet to 30.8 feet, 2. To reduce the required west interior side yard from 10 feet to 9.2 feet, within the R-2 Single Family Residence Zoning District.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

C. Discussion - Class D Liquor License - Amending the hours liquor can be served from a Class D Liquor License.

Mr. Dan Gombac, Director reported that Chuck's Southern BBQ Café requested changing the hours of when liquor can be served. He reported on the hours of the businesses in the area and stated that staff surveyed adjacent communities as well. Mr. Gombac reported that amending the license affects all of the businesses holding the Class D Liquor License.

The petition stated that he would like to change the hours in order to accommodate the working class as well as for weddings or banquets to visit the bar afterwards.

Mayor Kathy Weaver stated that she was not in favor of it.

There was discussion about setting a precedent and the Committee discussed and felt a separate license was more appropriate.

The petitioner stated that he has a full service menu seven days a week.

Alderman Belczak stated that this was a unique entity and that he doesn't have a huge problem with the morning but had concern regarding the late night hours.

Mr. Gombac questioned the petitioner's motivation for the change of hours.

The petitioner stated that this would allow him more wiggle room to accommodate customers that have early hours instead of late hours. He further stated that the gaming is tied to the Liquor License and that he would hate to see people go somewhere else. The petitioner stated that his Burbank business has 7:00 a.m. and that he hardly sells any liquor.

Mayor Weaver stated that extensions are not favorable and that she has an issue with 3:00 a.m.

The petitioner stated that the late time is not the most important to him.

Mayor Weaver stated that a new license could be created to go along with the 7:00 a.m. request but not the 3:00 a.m. request.

Mr. Gombac stated that he was concerned that Dotty's would have to be addressed. He stated that he would look into who is serving breakfast.

Chairperson Marchese stated that if Dotty's is serving breakfast then they have the ability to ask for the same.

The Committee agreed to a separate license and that the recommendation would go to the City Council for discussion and then back to the Committee for approval.

Mr. Gombac reported that he received an email from Resident Joe Weber in opposition of any changes in the late hours and was amicable to the early hours.

Mr. Gombac reported that the Committee recommendation will be to propose early hours only. There was no one in the audience wishing to present public comment.

- D. Resolution – Accepting a proposal from Associated Technical Services (ATS) for the 2015 Water Leak Survey, in an amount of \$8,761.89 for the Leak Detection Phase and per unit cost for the Leak Location Phase in the amount of \$395.00 per mainline or service leak, and \$95.00 per fire hydrant leak or mainline valve for a total amount not to exceed \$14,700.00.**

Mr. Dan Gombac, Director reported that this is a proposal from Associated Technical Services (ATS) for the 2015 Water Leak Survey. He reported that ATS was the lowest competitive bid.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese to approve a Resolution accepting a proposal from Associated Technical Services (ATS) for the 2015 Water Leak Survey, in an amount of \$8,761.89 for the Leak Detection Phase and per unit cost for the Leak Location Phase in the amount of \$395.00 per mainline or service leak, and \$95.00 per fire hydrant leak or mainline valve for a total amount not to exceed \$14,700.00.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

- E. Resolution - Authorizing the Mayor to accept a proposal from Superior Road Striping Co. in an amount not to exceed \$10,500.00 for the 2015 Road Striping Program.**

Mr. Dan Gombac, Director reported that is the awarded vendor for the Northwest Municipal Conference Joint Purchasing Cooperative for road striping to municipalities within the Midwest.

There was no one in the audience wishing to present public comment.

Alderman Marchese made a motion, and it was seconded by Alderman Belczak to approve a Resolution authorizing the Mayor to accept a proposal from Superior Road Striping Co. in an amount not to exceed \$10,500.00 for the 2015 Road Striping Program.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

- F. Resolution - Authorizing the purchase of six (6) various Steel Road Plates from Lee Jensen Sales Co. in an amount not to exceed \$10,360.00.**

Mr. Dan Gombac, Director reported that the road plates are used for excavating when a hole needs to temporarily be left open. He reported that it is better to own versus renting.

There was no one in the audience wishing to present public comment.

Alderman Marchese made a motion, and it was seconded by Alderman Belczak to approve a Resolution authorizing the purchase of six (6) various Steel Road Plates from Lee Jensen Sales Co. in an amount not to exceed \$10,360.00.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

G. Resolution - Authorizing the purchase of an Aluminum Trench Box from Lee Jensen Sales Co. in the amount of \$6,500.00.

Mr. Dan Gombac, Director reported that this is a safety related item. He reported that the pricing was negotiated.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese to approve a Resolution authorizing the purchase of an Aluminum Trench Box from Lee Jensen Sales Co. in the amount of \$6,500.00.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

H. Resolution - Accepting a proposal from United Septic, Inc. for Cleaning and Televising of Ironwood Avenue and Evergreen Lane Storm Sewer System in an amount not to exceed \$14,980 and Hourly Rate for Routine and Emergency Services for cleaning Storm Sewer Infrastructure in an amount not to exceed \$15,000 from May 2015-April 2016.

Mr. Dan Gombac, Director reported that this is for the cleaning and televising of Ironwood Avenue and Evergreen Lane storm sewer system. The proposal also included an hourly rate for routine and emergency services for cleaning storm sewer infrastructure.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese to approve a Resolution accepting a proposal from United Septic, Inc. for Cleaning and Televising of Ironwood Avenue and Evergreen Lane Storm Sewer System in an amount not to exceed \$14,980 and Hourly Rate for Routine and Emergency Services for cleaning Storm Sewer Infrastructure in an amount not to exceed \$15,000 from May 2015-April 2016.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

I. Minutes - April 27, 2015 Municipal Services Committee

Alderman Marchese made a motion, and it was seconded by Alderman Belczak to approve the April 27, 2015 Municipal Services Committee Meeting Minutes.

Upon voice vote, THE MOTION CARRIED unanimously 2-0.

DIRECTOR'S REPORT

None

NEXT SCHEDULED MEETING

Chairperson Marchese announced that the next regular meeting is scheduled for Monday, June 22, 2015 at 6:30 p.m.

ADJOURNMENT

With no further business before the Committee, Alderman Belczak made a motion and it was seconded by Alderman Marchese to adjourn. Upon voice vote, THE MOTION CARRIED unanimously and the meeting adjourned at 7:40 p.m.

RESPECTFULLY SUBMITTED:

**Joseph Marchese
Chairman**

**Tom Belczak
Alderman**

**CITY OF DARIEN
MUNICIPAL SERVICES COMMITTEE
June 22, 2015**

PRESENT: Joseph Marchese - Chairperson, Alderman Tom Belczak, Tom Chlystek, Dan Gombac – Director, Michael Griffith - Senior Planner, Elizabeth Lahey - Secretary

ABSENT: None

ESTABLISH QUORUM

Chairperson Marchese called the meeting of the Municipal Services Committee to order at 6:30 p.m. at the City Hall – City Council Chambers, Darien, Illinois and declared a quorum present.

NEW BUSINESS

A. Ordinance - Minor PUD Amendment to the Darien Towne Centre PUD Amvets Collection Center at Wal-Mart, 2189 75th Street: Location Change.

Mr. Dan Gombac, Director reported that the City Council granted a minor amendment to the PUD to allow an Amvets Collection Center to the west of Walmart. He reported that recently, Walmart required Amvets to move their collection center to the far northwest area of the parking lot to an underutilized area of the parking lot.

Mr. Gombac reported that staff tried to work with Walmart to an adjacent location as per the ordinance, but they were not in agreement. He stated that the proposed location is an out lot and the Darien Towne Center representatives were amicable to there were no problems or concerns for the out lot.

There was no one else wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Chlystek to approve an Ordinance - Minor PUD Amendment to the Darien Towne Centre PUD Amvets Collection Center at Wal-Mart, 2189 75th Street: Location Change.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

B. Resolution - Authorizing the purchase of a Kubota Model ZD326P-60 zero turn riding mower from Russo Power Equipment Inc. in the amount of \$11,998.00.

Mr. Dan Gombac, Director reported that this resolution authorizes the purchase of a turn riding mower to be utilized to maintain 33 acres of grass cutting and various right-of-ways. He reported that the 2006 Kubota would be declared surplus property and brought forward under a separate agenda memo.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Marchese approval of a Resolution authorizing the purchase of a Kubota Model ZD326P-60 zero turn riding mower from Russo Power Equipment Inc. in the amount of \$11,998.00.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

C. Resolution - Allowing the City of Darien to perform maintenance and emergency work on City owned infrastructure on State of Illinois maintained Right-of-ways for 2015 and 2016.

Mr. Dan Gombac, Director reported that this resolution is required by the State of Illinois and allows the City of Darien to perform maintenance and emergency work on City owned infrastructure on State of Illinois maintained right of ways right-of-ways for two years, 2015 and 2016.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Chlystek approval of a Resolution allowing the City of Darien to perform maintenance and emergency work on City owned infrastructure on State of Illinois maintained Right-of-ways for 2015 and 2016.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

D. Resolution – Accepting a quote from Advanced Automation and Controls Inc. in an amount not to exceed \$14,070 for the following:

Item 1 - Updating of Water System Computer Hardware, Software and Programming - \$8,720.

Item 2 - Plant 2- Programming Logic Controller for Variable Speed Pumps - Hardware and Programming - \$5,350.

Mr. Dan Gombac, Director reported that the existing computer and hardware was installed in 2008 operating on Windows XP which is no longer supported by Microsoft. He reported that the proposed purchase is for the computer hardware, software and programming. The system is a stand-alone system and is utilized to monitor and control the City's water system.

Mr. Gombac further reported that Item 2 is for updating the Supervisory Control and Data Acquisition (SCADA) system. The proposed Program Logic Controllers for the variable pumps will allow for optimal operation of the pumps. The improvement will allow for the pumps to operate at variable flow rates as the system demands. He reported that there will be additional cost saving opportunities as they relate to maintenance of equipment and energy.

Alderman Chlystek questioned staff is trained and if there is a warranty.

Mr. Gombac reported that there is a one-year warranty and the Water Plant Operators for the City are required to complete continue educating classes including SCADA operations.

There was no one in the audience wishing to present public comment.

Alderman Chylstek made a motion, and it was seconded by Alderman Marchese to approve a Resolution accepting a quote from Advanced Automation and Controls Inc. in an amount not to exceed \$14,070 for the following:

Item 1 - Updating of Water System Computer Hardware, Software and Programming - \$8,720.

Item 2 - Plant 2-Programming Logic Controller for Variable Speed Pumps - Hardware and Programming - \$5,350.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

E. Resolution - Authorizing the Mayor to execute a contract with Patriot Pavement Maintenance Inc. in an amount not to exceed \$132,368 for the 2015 Crack Fill Program.

Mr. Dan Gombac, Director reported that the resolution is for the 2015 Crack Fill Program. He reported that crack sealing is a routine maintenance treatment that delays roadway deterioration by sealing the water out of the pavement and base. Staff inventories the existing cracks approximately a year in advance and determines quantities for the crack seal program. The operation is completed on three year intervals or up to 9 years of treatment upon the resurfacing of a roadway. He further reported that severe winter conditions increases the cracks and adjusts the quantities to reflect the conditions.

Mr. Gombac reported that the references for Patriot Pavement were favorable.

There was no one in the audience wishing to present public comment.

Alderman Belczak made a motion, and it was seconded by Alderman Chylstek to approve a Resolution authorizing the Mayor to execute a contract with Patriot Pavement Maintenance Inc. in an amount not to exceed \$132,368 for the 2015 Crack Fill Program.

Upon voice vote, THE MOTION CARRIED unanimously 3-0.

DIRECTOR'S REPORT

Mr. Dan Gombac, Director reported that the City and the Darien Park District were denied a recent grant for Birchwood Park. He reported that Speedway and Brookhaven are working on an agreement for a cross access agreement. He stated that this will create optimal convenience between the two businesses and that Speedway would incur all costs.

Mr. Gombac reported that Darien Pointe has contracted with an outside broker for retail tenant agreements. The developers have reported there are many inquiries and no commitments. He reported that the owner of the former Phillips 66 is working with a major coffee brand retailer.

Alderman Marchese stated that the owner for Darien Pointe is not doing much for prospective businesses regarding the build out.

Mr. Gombac stated that City staff continue to work with the potential tenants.

Mr. Gombac reported that staff has been trying to schedule the painting of the speakers at Darien Pointe but there is too much rain. He also reported that the road program is behind because of the weather as well.

NEXT SCHEDULED MEETING

Chairperson Marchese announced that the next regular meeting is scheduled for Monday, July 27, 2015 at 6:30 p.m.

ADJOURNMENT

With no further business before the Committee, Alderman Belczak made a motion and it was seconded by Alderman Chlystek to adjourn. Upon voice vote, THE MOTION CARRIED unanimously and the meeting adjourned at 7:05 p.m.

RESPECTFULLY SUBMITTED:

**Joseph Marchese
Chairman**

**Tom Chlystek
Alderman**

**Tom Belczak
Alderman**